

**WENATCHEE PLANNING COMMISSION
SCHEDULED MEETING
October 23, 2024
WENATCHEE CITY HALL COUNCIL CHAMBERS
301 Yakima Street
Wenatchee, WA 98801
5:30 PM**

AGENDA

I. CALL TO ORDER AND WELCOME

II. ADMINISTRATIVE AFFAIRS

A. Approval of the minutes from the last regular meeting on September 18, 2024

III. PUBLIC COMMENT PERIOD

Comments for any matters not included on the agenda

IV. OLD BUSINESS

None

V. NEW BUSINESS

A. Workshop: Middle Housing & ADU Code Updates

VII. ADJOURNMENT

Wenatchee City Hall is open for the public to attend meetings in person. If you have questions about participating remotely in the meeting, please contact Planning Department staff at (509) 888-3286 at least 24 hours in advance.

In compliance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting, please contact the Mayor's office at (509) 888-6204 (TTY 711). Notification 72 hours prior to the meeting will enable the City to make reasonable arrangements to ensure accessibility to the meeting (28 CFR 35.102-35.104 ADA Title 1.)

MINUTES

I. CALL TO ORDER

Chair Rani Sampson called the meeting to order at 5:30p.m., with the following members in attendance: Ruby Gaston, Amber Bollinger, Joe Gamboni, Anupama Vembar, and Susan Albert. Josh Jorgensen was absent.

City Planning staff was represented by: Stephen Neuenschwander, Assistant Director; Ruth Traxler, Principal Planner; Matt Parsons, Senior Planner; Mia Bretz, Associate Planner; Thomas Stanford, Development Review Engineer; and Eva Osburn, Administrative Assistant.

II. ADMINISTRATIVE AFFAIRS

A. Approval of the minutes from the regular meetings on August 21, 2024.

Commissioner Joe Gamboni moved to approve the minutes from the August 21 regular meeting. Commissioner Ruby Gaston seconded the motion. The motion carried unanimously.

III. PUBLIC COMMENT PERIOD

There was no public comment.

IV. OLD BUSINESS

None

V. NEW BUSINESS

A. Workshop: Draft Amendments to Title 10

Stephen Neuenschwander, Assistant Director, presented.

B. Workshop: Middle Housing & ADU Code Updates

Mia Bretz, Associate Planner; Matt Parsons, Senior Planner, Ruth Traxler, Principal Planner; and Thomas Stanford, Development Review Engineer; presented.

VI. OTHER

A. October Planning Commission meeting changed from October 16 to October 23 due to the APA Conference.

VII. ADJOURNMENT

With no further business to come before the Planning Commission, Chair Rani Sampson adjourned the meeting at 6:46p.m.

Respectfully submitted,
CITY OF WENATCHEE
COMMUNITY DEVELOPMENT DEPARTMENT
Eva Osburn, Administrative Assistant



**City of
Wenatchee**

Community Development Department

City Hall

301 Yakima Street, Suite 100

Wenatchee, WA 98801

(509) 888-3200

Fax (509) 888-3201

TO: Planning Commission

FROM: Community Development Staff

SUBJECT: Housing Workshop

DATE: October 16, 2024

ATTACHMENTS:

- Title 10 code changes for middle housing, ADUs and other zoning codes
- Residential Design Guidelines changes for middle housing applicability
- Amendments to the Comprehensive Plan
- Capital Facilities Plan

Intro

After spending the year discussing middle housing and accessory dwelling unit (ADU) code changes, staff will bring the final draft of code before Public Hearing. This workshop is a great opportunity for the Planning Commission to dive into any remaining questions you may have about these updates: How do the new middle housing standards compare to existing housing standards? Why are there edits to all of the residential code sections? Does the density increase apply to all lots? What code changes are due to state law and which are due to City choice? There are a lot of edits to the Residential Design Guidelines, are any of them substantive?

Please consider our discussions over the last year and be ready to bring your lingering questions to our workshop!

Changes Since September

Here is a summary of the few changes to the draft code since the September meeting.

Parking in Front

Currently off-street parking must occur to the side or rear of buildings, except for single-family homes and duplexes. Since we are combining development standards for all housing types with 1-4 units on one lot, we are reconsidering how parking in front of a building should occur in this case.

In September staff presented the WA Commerce recommendation of allowing parking in the front of buildings for 1-4 units, but restricting that parking area/driveway to a maximum width (60% of the length of the street frontage property line). After some research, staff determined that measuring width appeared to be less consistent to apply than measuring area.

Staff is now recommending allowing parking in front of buildings for 1-4 units with a restriction on the paved area. Utilizing a code section borrowed from the City of Spokane (who has found success in

implementation), this updated code section 10.47.035(2)(g) better limits large paved areas in the front yard, accommodates an 18-foot-wide driveway, and allows exceptions for unique properties.

Comprehensive Plan Amendments

Staff added some amendments to the residential portion of the Land Use Implementation Chart located in the Land Use Element of the Wenatchee Urban Area Comprehensive Plan to ensure the proposed code amendments align with it. The proposed code amendments are well supported in terms of following the direction laid out in various goals and policies but the recent legislation moved things in a slightly different direction than was anticipated so some minor changes are necessary.

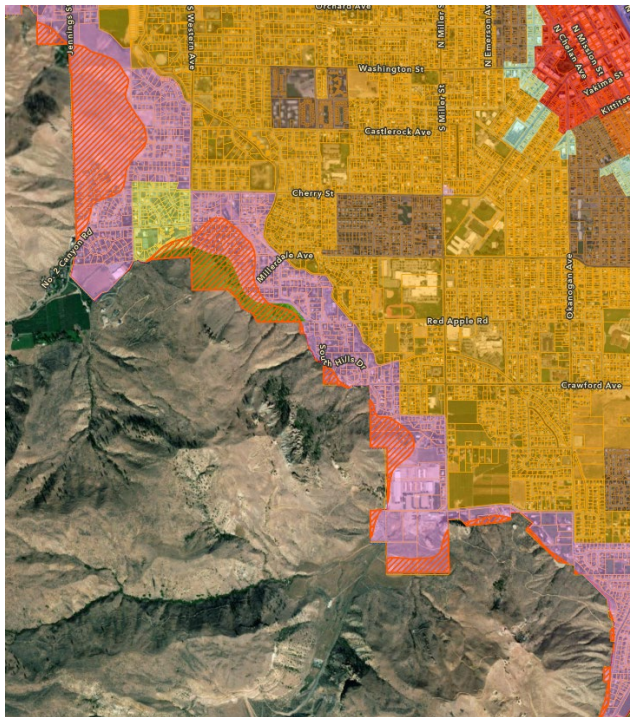
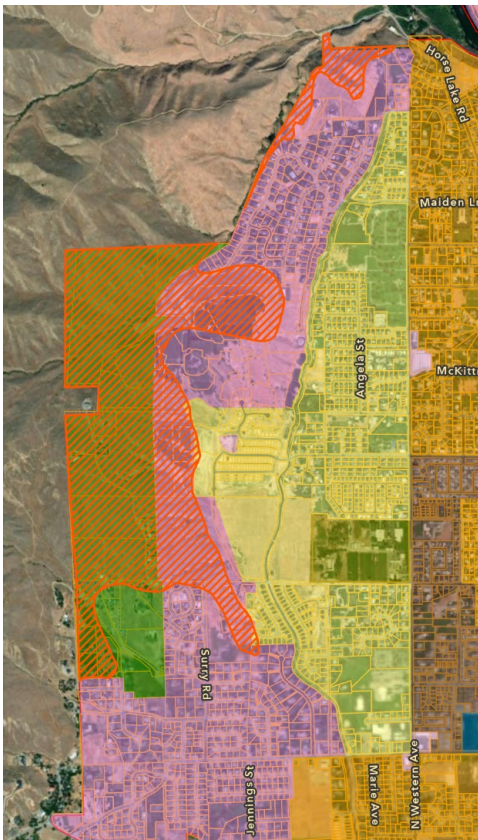
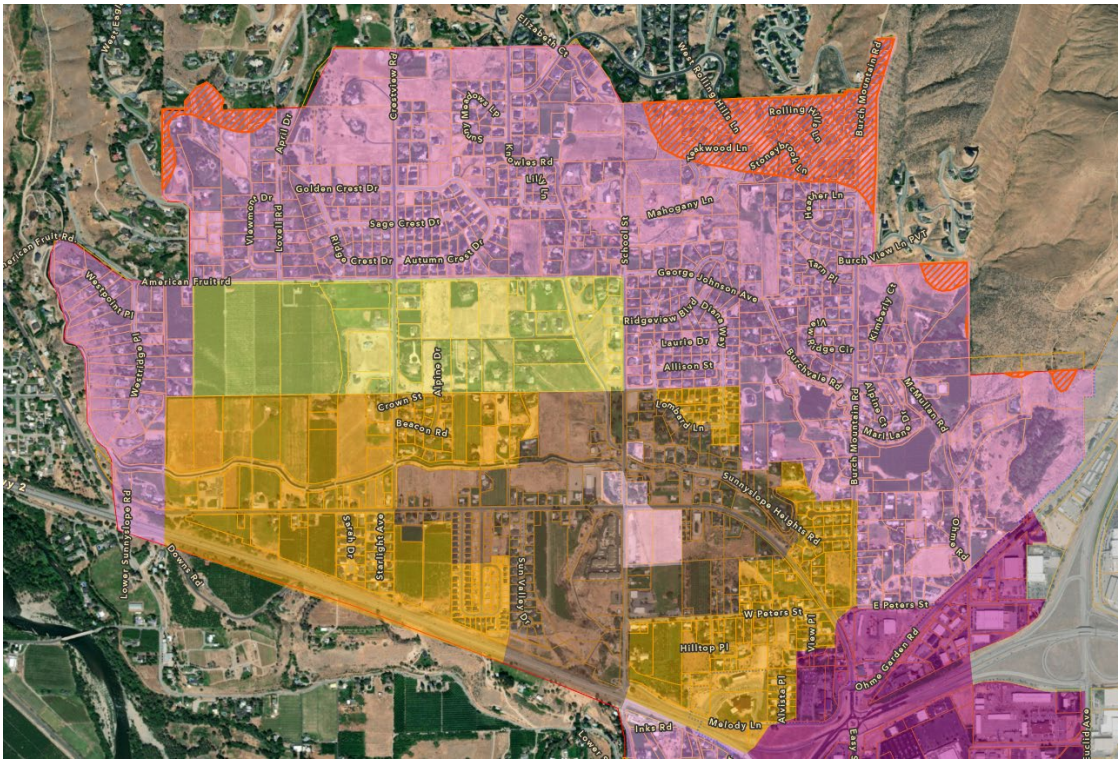
Capital Facilities Plan Amendments

City staff will provide a high-level overview the amendments to the Capital Facilities Plan. The Capital Facilities Plan is tied directly to the annual City budget and details expenditures, timelines, and priorities for capital projects or facility needs.

Density Exemption for Mule Deer Habitat

As discussed at the September meeting, the current set of amendments utilizes a provision allowing for portions of parcels located within Rocky Mountain Mule Deer Habitat to only utilize the existing units per acre density standard and not be subject to the proposed units per lot standard. At the time of the September meeting, the proposed code language had not been finalized. See the proposed Section 10.46.020 and 10.46.110 for the current proposed language.

Below, images of the Rocky Mountain Mule Deer Habitat (red/orange hatch) is overlaying the zoning map in the areas where it exists.



Timeline for Middle Housing Compliance

Under the state statute the city must adopt compliant regulations by December 31, 2026 or the statewide [model ordinance](#) developed by the Washington State Department of Commerce will govern as described in [RCW36.70A.636\(2\)](#). However, the City has been awarded a grant for this work and under that program we have agreed to adopt compliant provisions by the end of 2024. Early adoption of housing standards to comply with the legislative updates will assist the city in meeting the housing demand by providing a simplified development code for middle housing types.

- 60-Day Review is October 4, 2024 – December 3, 2024
- Planning Commission public hearing – November 20, 2024
- City Council public hearing and potential adoption – December 5, 2024
- The effective date of the ordinance, if adopted, would most likely be in mid-January 2025.

Questions?

If you have any questions, please do not hesitate to contact us:

- Mia Bretz, Associate Planner (509) 888-3250 MBretz@WenatcheeWA.Gov
- Matt Parsons AICP, Senior Planner (509) 888-3253 mparsons@wenatcheewa.gov
- Stephen Neuenschwander, Assistant Director (509) 888-3285
SNeuenschwander@WenatcheeWA.Gov

Middle Housing Code

DRAFT

Contents

- 10.08 Definitions..... 2
- 10.10.020 District use chart..... 4
- Chapter 10.46 DEVELOPMENT STANDARDS CHARTS 5
 - 10.46.020 Residential district development chart..... 5
 - 10.46.080 Setback measurements and exceptions. 6
 - 10.46.110 Density calculations and exceptions..... 6
- Chapter 10.47 RESIDENTIAL USE STANDARDS..... 8
 - Sections:..... 8
 - 10.47.035 One to four dwelling units. [NEW SECTION]..... 8
 - 10.47.040 Accessory dwelling units (ADU). 11
 - 10.47.050 Single-family..... 14
 - 10.47.060 Single-family – Cluster. 17
 - 10.47.090 Cottage..... 18
 - 10.47.100 Duplex. 23
 - 10.47.120 Courtyard. 24
 - 10.47.130 Townhouse..... 28
 - 10.47.150 Multifamily..... 32
- Chapter 10.48 GENERAL DEVELOPMENT REGULATIONS..... 36
 - 10.48.020 Driveways and Access. 36
 - 10.48.040 Pedestrian connections. 37
 - 10.48.120 Vehicle parking..... 38
- Chapter 10.60 OFF-STREET PARKING..... 38
 - 10.60.030 General requirements..... 38
 - 10.60.080 Off-street parking requirements..... 38
- Chapter 10.65 CONDITIONAL USES..... 40
 - 10.65.220 Public utilities and services. 40
- Title 15 Development Impact Fee..... 41
 - 15.02.040 Exemptions. 41
- Title 7 STREETS AND SIDEWALKS 41

10.08 Definitions

10.08.040 "A."

~~"Accessory dwelling unit" see "Dwelling, accessory," means a dwelling unit that has been added onto, created within, or separated from a single-family detached dwelling for use as a complete independent living unit with provisions for cooking, sanitation and sleeping. Accessory dwelling units are subject to the provisions of WCC 10.47.040.~~

10.08.050 "C."

~~"Cottage housing" refers to small and mostly detached individual dwelling units. Lots where two to three cottage houses are proposed (referred to as "infill cottage housing") are subject to the provisions of WCC 10.47.080. Lots where four or more cottage houses are proposed are subject to the provisions of WCC 10.47.090.~~

~~"Courtyard housing" means small multifamily units arranged around a courtyard and complying with the provisions of WCC 10.47.120.~~

10.08.055 "D."

~~"Duplex" means a detached residential building designed for occupancy by two self-contained attached dwelling units living independently of each other. Duplexes are subject to the provisions of WCC 10.47.100.~~

~~"Driveway" means an area of a private property that provides vehicular access from the street to carports, garages, or parking space(s) as allowed by WCC Chapters 10.48 and 10.60.~~

"Dwelling" or "dwelling unit" means one or more rooms designed as complete independent living facilities for one or more persons and includes permanent provisions for living, sleeping, cooking (a kitchen including a sink, stove or range, refrigerator, and countertop(s)), and sanitation (a bathroom containing a toilet, shower or bathtub, and sink).

"Dwelling, accessory" also called an ADU or DADU, means a dwelling unit that has been added onto, created within, or separated from a primary dwelling unit on the same lot.

"Dwelling, duplex" means two attached dwelling units.

"Dwelling, cottage" means dwelling units with a maximum square footage of 1,200 square feet and a common open space that either (a) is owned in common; or (b) has units owned as condominium units with property owned in common and is a minimum of 200 square feet per unit. Cottages may be in single-family or duplex form. Density is calculated at 0.5 per unit.

"Dwelling, courtyard" means three or more multifamily dwelling units arranged on two or three sides of a yard or court which is preserved as common open space. Density may be reduced to 0.75 per unit for units 1,000 square feet or less.

~~“Dwelling, live-work dwelling” means a dwelling unit designed to accommodate a small commercial enterprise on the ground floor and a residential unit above and/or behind. See WCC 10.47.140 for applicable standards.~~

~~“Dwelling, multifamily” means three or more attached residential dwelling units, on one lot or parcel. Multifamily developments are subject to the provisions of WCC 10.47.150.~~

~~“Dwelling, single-family” means a permanent single detached residential living dwelling unit, containing sufficient facilities to function as an independent dwelling unit. Single-family dwellings may be site built or manufactured.~~

~~“Dwelling, single family – cluster” means a single-family development featuring reduced lot sizes within a subdivision; provided, that open space is integrated into the design to compensate for the clustered lot arrangement. Two design options apply to single-family cluster developments as set forth in WCC 10.47.060.~~

~~“Dwelling, single family – courtyard” means a single family development served by a shared private lane and complying with the provisions of WCC 10.47.070.~~

~~“Dwelling, townhouse” means a row of at least three attached dwelling units, constructed to a townhouse standard per International Residential Code (IRC) R302 as amended. Each unit has its own front access to the outside, no unit is located over another unit, and each unit is separated from any other unit by one or more common walls.~~

10.08.095 “L.”

~~“Live-work dwelling” means a dwelling unit designed to accommodate a small commercial enterprise on the ground floor and a residential unit above and/or behind. See WCC 10.47.140 for applicable standards.~~

10.08.100 “M.”

~~“Major transit stop” means a stop on a high capacity transportation system funded or expanded under the provisions of chapter 81.104 RCW, commuter rail stops, stops on rail or fixed guideway systems, stops on bus rapid transit routes, or stops for a bus or other transit mode providing actual fixed route service at intervals of at least fifteen minutes for at least five hours during the peak hours of operation on weekdays (including those funded for development and projected for construction within an applicable six-year transit plan under RCW 35.58.2795).~~

10.08.115 “P.”

~~“Primary structure” means a building that is occupied or is intended to be occupied by a primary use, including an attached garage. When a dwelling unit (pursuant to WCC 10.08.055) is the primary structure on a parcel, it may be termed a “primary dwelling unit.”~~

~~“Public utilities and services” means: -equipment installations for utility and service purveyors including, but not limited to, telephone exchanges, electrical substations, water reservoirs, pump stations, and similar facilities of service providers, specifically excluding diesel, natural gas and similar internal combustion engine generated power production.~~

(1) Public or private infrastructure services that need to be located in or near the area where the service is provided and generally do not have regular employees or customers at the site. Common infrastructure services may include water and sewer pump stations; sewage disposal and conveyance systems; electrical substations; water towers and reservoirs; water quality and flow control facilities; water conveyance systems; stormwater facilities and conveyance systems; and telephone exchanges. Exceptions are services for the express purpose of transmitting or transporting electricity, gas, oil, water, sewage, communication signals or other similar services on a regional level such as a rail line or utility corridor.

(2) Public safety and transportation facilities that need to be located in or near the area where the service is provided. Common facilities include mass transit stops or turnarounds, transit centers, park-and-ride facilities for mass transit; and public safety facilities, including fire and police stations and emergency communication broadcast facilities.

Accessory uses may include parking; control, monitoring, data or transmission equipment; and holding cells within a police station. Public utility and service offices where employees or customers are generally present are classified as “office, business.” Bus barns are classified as “maintenance and service facilities for motor vehicle passenger transportation.”

10.08.135 “T.”

~~“Townhouse” means a row of at least three attached housing units, constructed to a townhouse standard per International Residential Code (IRC) R302 as amended. Each unit has its own front access to the outside, no unit is located over another unit, and each unit is separated from any other unit by one or more common walls. Townhouse design standards are set forth in WCC 10.47.130.~~

10.10.020 District use chart.

Uses	Commercial				Mixed			Residential				Overlay								
	CBD	NWBD	SWBD	CN	CD	I	WMU	OMU	RMU	RF	RS	RL	RM	RH	HEO	CSO	MRC	IO	PO	RRO
Single-family dwelling (WCC 10.47.050)	P ₁₀	P ₁₀	P ₁₀	P ₁₀	~	~	~	P	P	P	P	P	P	P	~	~	P	~	~	~
Single-family cluster (WCC 10.47.060)	~	~	~	~	~	~	~	~	~	P	P	P	~	~	~	~	~	~	~	~
Single-family courtyard (WCC 10.47.070)	~	~	~	~	~	~	~	~	~	~	P	P	P	~	~	~	~	~	~	~
Cottage housing (WCC 10.47.080 — 10.47.090)	~	~	~	~	~	~	~	P	P	P _~	P	P	P	P	~	~	P	~	~	~
Duplex (WCC 10.47.100)	~	P ₁₀	P ₁₀	P ₁₀	~	~	~	P	P	P	P	P	P	P	~	~	P	~	~	P

Patio homes (WCC 10.47.110)	~	~	~	~	~	~	~	~	P	P	P	P	P	P	~	~	P	~	~	P
Courtyard housing (WCC 10.47.120)	~	~	P	~	~	~	~	P	P	P ₁₆	P ₁₆	P ₆	P	P	~	~	P	~	P	P
Townhouses (WCC 10.47.130)	~	~	P	P ₁₂	~	~	~	P	P	P ₁₆	P ₁₆	P ₆	P	P	~	~	P	~	P	P
Live-work dwellings (WCC 10.47.140)	P ₁₁	P	P	P	~	~	P	P	P	~	~	~	~	~	P ₁₁	P ₁₁	P	~	P	P
Multifamily dwellings (WCC 10.47.150)	P ₁₁	P	P	P ₁₂	~	~	P	P	P	P ₁₆	P ₁₆	P ₁₆	P	P	P ₁₁	P ₁₁	P	~	P	P
Mixed use building (WCC 10.47.160)	P ₁₁	P	P	P	~	~	P	P	P	~	~	~	~	~	P ₁₁	P ₁₁	P	~	P	P
Accessory structure, residential (WCC 10.48.030)	~	~	~	~	~	~	P	P	P	P	P	P	P	P	~	~	P	~	P	P
Accessory dwelling unit (ADU) (WCC 10.47.040)	P ₁₄	P ₁₄	P ₁₄	P ₁₄	~	~	~	P	P	P	P	P	P	P	~	~	P	~	~	~

⁶ Townhouses and courtyard housing in this zone are limited to no more than four attached units in one structure.

¹⁰ Only such uses in existence as of November 7, 2019, are considered a permitted use.

¹⁴ Only permitted as an accessory use to a single-family dwelling in existence as of November 7, 2019, attached units in one structure.

¹⁶ Referenced dwelling types in this zone are limited to no more than three attached units in one structure.

Chapter 10.46 DEVELOPMENT STANDARDS CHARTS

10.46.020 Residential district development chart.

Standard	RS	RL	RM	RH	RF	Conditions/ Exceptions/ Reference
Lot Area	7,250 sf, except 10,000-sf	5,500 sf, except 8,000-sf	3,000 sf, except 4,500-sf	3,000 sf, except 4,000-sf	10,000 sf, except 15,000-sf	WCC 10.46.060

	for-a duplex	for-a duplex	for-a duplex	for-a duplex	for-a duplex	
Maximum Density	6 dwelling units per acre(x)	8 dwelling units per acre(x)	20 dwelling units per acre(x)	40 dwelling units per acre(x)	4 dwelling units per acre(x)	WCC 10.46.110 <u>(x) two dwelling units per lot is allowed unless otherwise provided in WCC10.46.110</u>

10.46.080 Setback measurements and exceptions.

(3) Rear Setbacks.

(d) Accessory structures may observe a five-foot setback in the rear setback provided:

- (i) The structure is not greater than ~~24~~0 feet in height.
- (ii) No projections (eaves, etc.) are within three feet of a property line.
- (iii) No combination of structures extends more than 50 percent of the horizontal distance of the rear property line.

(10) Setbacks for garages served by a shared driveway. The minimum setback for a garage facing a shared driveway is at least 20 feet from either the edge of pavement or back of sidewalk, whichever is closer to the garage.

10.46.110 Density calculations and exceptions.

(1) Calculations for Determining Maximum Density, Where Expressed in Dwelling Units per Acre. The entire gross site area may be used in the calculation of the maximum allowed residential density including half of the adjacent city or county right-of-way (to the centerline).

(2) How to Calculate Density. Maximum density for an individual site shall be calculated by multiplying the site area (in acres) by the maximum dwelling units/acre in the applicable zone. When calculation results in a fraction, the fraction shall be rounded to the nearest whole number as follows:

- (a) Fractions of 0.50 and above are rounded up.
- (b) Fractions below 0.50 are rounded down.

(3) Exceptions to Density Standards. Exceptions to established density standards shall be granted in the following instances:

- (a) When an exception is granted for a particular use described in Chapter 10.47 WCC.

(b) Additional density may be granted for affordable housing pursuant to RCW 35A.63.300 when local needs can be demonstrated compliant with the comprehensive plan.

(c) Portions of a lot designated as both Rocky Mountain Mule Deer Habitat (pursuant to Chapter 12.08 WCC) and Wildland Urban Interface Primary Zone (pursuant to Chapter 3.36 WCC) shall only be subject to the units per acre standard listed in Section 10.46.020 WCC and not subject to the units per lot standard described in that section.

(d) Up to four units per lot are allowed for lots 1,000 square feet or more in RF, RS, RL, RM and RH when they are within one-quarter mile walking distance of a major transit stop or the following standards are met, except on portions of a lot designated as both Rocky Mountain Mule Deer Habitat (pursuant to Chapter 12.08 WCC) and Wildland Urban Interface Primary Zone (pursuant to Chapter 3.36 WCC).

(i) The applicant shall commit to renting or selling at least one unit as affordable housing. Dwelling units that qualify as affordable housing shall have costs, including utilities other than telephone, that do not exceed 30 percent of the monthly income of a household whose income does not exceed the following percentages of median household income adjusted for household size, for the county where the household is located, as reported by the United States Department of Housing and Urban Development:

(A) Rental housing: 60 percent.

(B) Owner-occupied housing: 80 percent.

(ii) The units shall be maintained as affordable for a term of at least 50 years, and the property shall satisfy that commitment and all required affordability and income eligibility conditions. The applicant shall record a covenant or deed restriction that ensures the continuing rental or ownership of units subject to these affordability requirements consistent with the conditions in chapter 84.14 RCW for a period of no less than 50 years.

(iii) The covenant or deed restriction shall address criteria and policies to maintain public benefit if the property is converted to a use other than that which continues to provide for permanently affordable housing.

(iv) The units dedicated as affordable housing shall:

(A) Be provided in a range of sizes comparable to other units in the development.

(B) The number of bedrooms in affordable units shall be in the same proportion as the number of bedrooms in units within the entire development.

(C) Generally, be distributed throughout the development and have substantially the same functionality as the other units in the development.

Chapter 10.47 RESIDENTIAL USE STANDARDS

Sections:

- 10.47.010 Purpose.
- 10.47.015 Applicability.
- 10.47.020 Residential structures.
- 10.47.030 Residential Design Guidelines.
- 10.47.035 One to four dwelling units. [NEW SECTION]
- 10.47.040 Accessory dwelling units (ADU).
- 10.47.050 Single-family ~~dwelling~~s.
- 10.47.060 Single-family – Cluster.
- ~~10.47.070 Single family – Courtyard.~~
- ~~10.47.080 Infill cottage housing (two to three units).~~
- 10.47.090 Cottage ~~housing (four or more units).~~
- 10.47.100 Duplexes.
- ~~10.47.110 Patio homes.~~
- 10.47.120 Courtyard ~~housing~~.
- 10.47.130 Townhouses.
- 10.47.140 Live-work ~~dwelling~~s.
- 10.47.150 Multifamily ~~development~~.
- 10.47.160 Mixed use building.
- ~~10.47.170 Alley access and improvements.~~
- 10.47.180 Manufactured home communities.

10.47.035 One to four dwelling units. [NEW SECTION]

Dwelling units defined herein and where allowed by Chapter 10.10 WCC, District Use Chart, shall meet the following standards unless otherwise regulated within this code:

(1) Purpose

- (a) To enhance the character of the street and neighborhood and maintain “eyes on the street” for safety to pedestrians and to create a more welcoming and interesting streetscape.
- (b) To enhance the streetscape and promote neighborhood interaction and safety through design.
- (c) To deemphasize garages and driveways as major visual elements along the street.
- (d) To provide usable open space for residents.
- (e) To provide housing types that are responsive to changing household demographics (e.g., retirees, small families, young professionals), provide opportunities for more affordable housing within single-family neighborhoods, and provide opportunities for infill development consistent with goals of the Wenatchee urban area comprehensive plan.
- (f) To ensure compatibility with neighboring uses.

(2) Standards

(a) Applicability. The standards of this section shall apply to the development of up to four dwelling units on one lot, including new buildings, building conversions, additions or modifications.

(b) Form. Dwelling units may be detached, attached, stacked, row, or otherwise combined. Allowable unit types include single-family, duplex, cottage, courtyard, multifamily, and townhouse.

(c) Building Design

(i) All facades that are visible from the street and that are not separated from the street by a dwelling or located more than 100 feet from a street must have the following elements:

(A) Entry. A primary entrance that is oriented to the street shall be provided.

(B) Windows and doors. A minimum of 15 percent of the area of the street-facing façade elevation shall include windows or doors. Garage doors are calculated as part of the façade area (they do not count as a door), but windows on garage doors count toward the window area.

(ii) Weather Protection. All new dwellings shall provide a covered pedestrian entry.

(A) For entries serving a single dwelling, facing the street and not separated from the street by a dwelling or located more than 100 feet from a street, a minimum weather protection of four feet wide by three feet deep shall be provided.

(B) For all other entries serving a single dwelling, a weather protection at least two feet deep shall be provided.

(C) For all primary building entries that serve more than one residence, a minimum weather protection of at least six feet wide and five feet deep shall be provided.

(iii) Articulation. Buildings over 26 feet high shall apply at least one articulation method every 30 feet for facades.

(A) Roofline change or a roof dormer with a minimum of four feet in width.

(B) Balcony(ies) with a minimum of 50 square feet and accessible from an interior room.

(C) An offset of the façade of a minimum of two feet in depth.

(D) A roofed porch at least 50 square feet in size for facades facing the street.

(E) Windows and doors are at least 30 percent of the area for facades facing the street.

(F) Meet the requirements of façade articulation methods in the Residential Design Guidelines E.3.1.c.

(d) Open Space. All new dwelling units shall provide open space, that is usable for human activity, of 100 square feet per dwelling unit or 10 percent of the dwelling unit(s) floor area.

(i) Exterior ground level open space must be accessible to each unit either in common or private areas with a minimum dimension of 10 feet on all sides. Drive aisles and parking areas shall not count in the calculations for open space.

(ii) Building related open space may be provided in the form of balconies, roof decks or interior spaces and only count as usable open space when they meet the requirements in Residential Design Guidelines C.3.2.

(e) Greenery.

(i) All new dwelling units shall adhere to the Street Trees section of the Landscaping Chapter 10.62.100(6) WCC.

(ii) Fifty percent of the area of the street setback must be planted and maintained utilizing one of the methods below. A patio or porch may be included in the calculation of planted area. Use of landscape structures such as trellises, raised beds and fencing to unify the overall site design is encouraged. The front planted area may be counted towards required open space.

(A) Planted with living landscape material, such as ground cover, shrubs or trees.

(B) Landscaped to the street frontage standard, per WCC 10.62.100(2).

(f) Parking ~~must be located according to WCC 10.60.030(7), except up to four vehicles may be between the building and the street when in a designated parking area that is not more than 20 feet wide.~~ Within one half-mile walking distance of a Major Transit Stop, no parking is required.

(g) Driveways and off-street parking spaces may be located between a building and a street when either of the following conditions are met:

i. In the RL, RS and RF zones, up to forty percent of the land area between the street lot line and the closest point of the building may be paved or used for vehicle parking. In addition, on corner lots, no more than twenty percent of the land area between the side street lot line and the side street building line may be paved or used for vehicle parking. As an exception to the area limitations in this paragraph, a lot is allowed a maximum 18-foot wide driveway and vehicle parking.

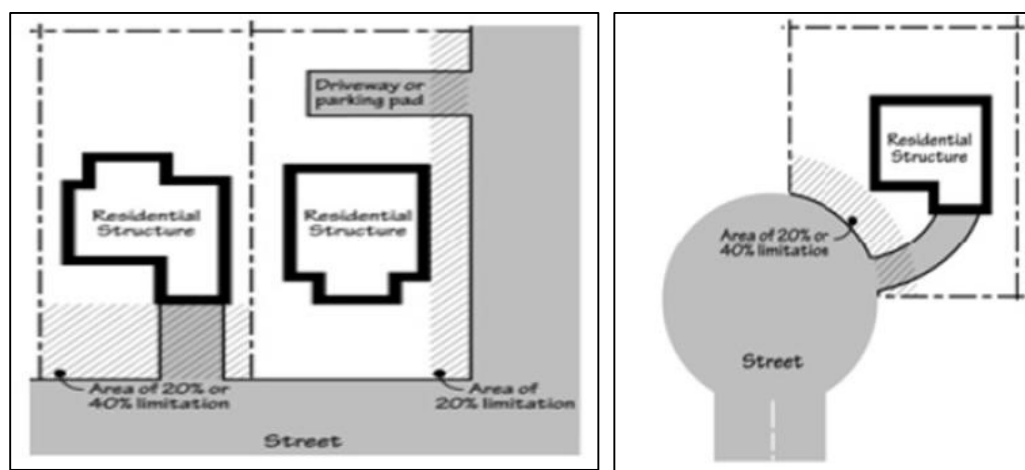
ii. In the RM and RH zones, up to twenty percent of the land area between the street lot line and the closest point of the building may be paved or used for vehicle parking. In addition, on corner lots no more than twenty percent of the land area between the side street lot line and the side street building line may be paved or used for vehicle parking. As an exception to the area limitations in this

paragraph, a lot is allowed a maximum 18-foot wide driveway and vehicle parking.

iii. Exceptions.

(A) Driveway coverage in the residential zones may exceed the size limitations of (4)(a) and (4)(b) when the subject property is located on a principal arterial and the increase in site coverage is due to inadequate maneuvering area for the safe exit of vehicles from the site. The exception to driveway coverage is reviewed by the engineering department as a portion of the driveway access permit.

(B) When paved pedestrian connections are separated from driveways and parking areas, they are not included in the calculation of paved area.



(h) Conversions. When existing residential buildings are expanded or converted to a new residential type, only those portions of the expanded or converted development shall meet the standards in this section.

10.47.040 Accessory dwelling units (ADU).

All accessory dwelling units, where defined in WCC [10.08.040055](#) and where allowed by Chapter [10.10](#) WCC, District Use Chart, shall meet the following standards unless otherwise regulated within this code:

(1) Purpose. ~~The purpose of this section is:~~

(a) To regulate the establishment of accessory dwelling units within or in conjunction with ~~single-family a primary dwellings unit while preserving the character of single-family neighborhoods;~~

(b) To make it possible for adult children to provide care and support to a parent or other relatives in need of assistance, or to provide for the care of disable persons within their own homes;

~~(c) To provide increased security and companionship for homeowners;~~

~~(cd) To provide the opportunity for homeowners to gain the extra income necessary to help meet the rising costs of home ownership; or to provide for the care of disabled persons within their own homes.~~

(d) To provide more flexible housing options and more diverse housing types.

~~(2) Standards for All Accessory Dwelling Units, Including Those Attached to the Main Residence and Those That Are in a Separate Building but on the Same Property.~~

(a) Form. ~~An ADUs~~ may be created within, or detached from, any existing or new ~~single-family primary~~ dwelling unit as a subordinate use. Dwelling units that are physically separated from the primary structure, that is, in a separate building, are “detached accessory dwelling units” (DADUs). Detached accessory dwelling units may share a common wall in a duplex arrangement.

(b) ~~Number Density.~~ No more than one Two ADUs may be created per ~~legal lot of record,~~ where allowed by WCC 10.10, District Use Chart.

~~(c) Occupancy.~~ Only the property owner, which shall include title holders and contract purchasers, may apply for an ADU. For properties in the RS, RL, and RF zones, the property owner shall occupy either the primary dwelling or the ADU as their principal residence for at least six months of the year.

~~(cd) Minimum Parking.~~ A minimum of ~~three one~~ parking spaces shall be provided for ~~the principal and each~~ accessory dwelling units. On nonarterial streets where on-street parking is available ~~(as approved by the city engineer)~~ abutting the lot, the city engineer may allow for on-street parking to satisfy the requirement for up to one parking space per ADU ~~only two off-street spaces shall be provided for the principal and accessory dwelling units.~~ Within one half-mile walking distance of a Major Transit Stop, no parking is required for ADUs.

~~(de) Maximum Size.~~ In no case shall an ADU be larger than ~~50 percent of the habitable floor area of the principal dwelling, nor more than 800-1,000~~ square feet of gross floor area.

~~(f) Living Facilities.~~ The ADU shall include, at a minimum, kitchen, bathroom and sleeping facilities.

~~(eg) Entrance Location.~~ If a separate outside entrance is necessary for an ADU that faces a street, the entry shall include weather protection of at least four feet wide by three feet deep located within the primary dwelling, that entrance shall be located either on the rear or side of the building.

~~(fh) Home Occupations.~~ A home occupation may be permitted in either the primary dwelling or the ADU but not in both if otherwise allowed in the WCC.

~~(i) Utilities.~~ Utilities for an ADU shall be metered jointly with the primary dwelling except as otherwise required by ordinance.

~~(gj) Addressing.~~ An address shall be assigned by the city at the time of building permit issuance.

~~(k) Covenants.~~ The property owner shall file a restrictive covenant with Chelan County's auditor's office for the subject property prior to final building inspection approval for the ADU. The recorded information shall:

~~(i) Identify the address of the property;~~

~~(ii) State that the owner(s) must reside in either the principal or accessory dwelling unit if located in the RS, RL, or RF zoning districts;~~

~~(iii) State that the owner(s) will notify any prospective purchasers of the limitations of the ADU;~~

~~(iv) State that the ADU shall not be subdivided or otherwise segregated in ownership from the primary dwelling unit; and~~

~~(v) Provide for the removal of the ADU if any of the requirements of this title are violated.~~

~~(f) Usable Open Space. No additional open space is required for the ADU or DADU. Lots containing an ADU or DADU must demonstrate compliance with the open space standards for single-family residences as set forth in WCC 10.47.050(2)(e) before an ADU or DADU can be legally established.~~

~~(h) Existing Structures. [AS OF DATE OF THIS ORDINANCE], existing legal non-conforming accessory structures, including detached garages, may be converted to an ADU even if they do not conform to current code requirements for setbacks or lot coverage.~~

(3) Standards for ~~All~~ Detached Accessory Dwelling Units (DADU).

(a) Setbacks. DADUs shall meet rear and side accessory structure setback requirements and shall be set back equal to or further than the primary dwelling from the front property line.

(b) Maximum Height. Maximum height for a DADU is limited to 24 feet.

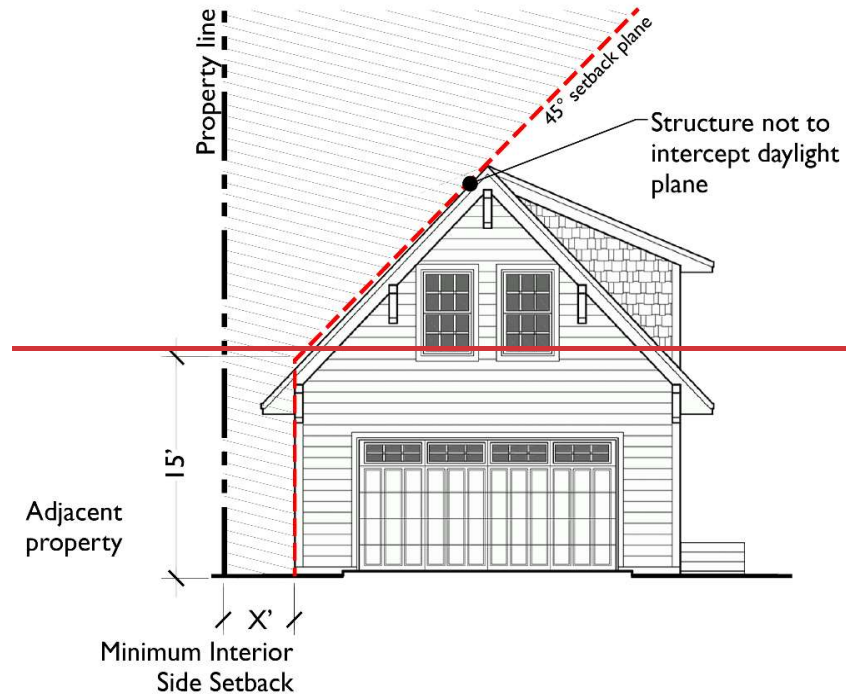
~~(i) DADUs with a roof pitch of 6:12 or greater: 25 feet.~~

~~(ii) DADUs with a roof pitch of less than 6:12: 20 feet.~~

~~(iii) Special Side Setback Standards for DADUs. From a height of 15 feet at the minimum side setback, DADUs shall step back at a 45-degree angle away from the side property line as shown in Figure 10.47.040(3) below. Structures set back further than the minimum side setback shall not encroach into the setback/daylight plane.~~

~~Figure 10.47.040(3)~~

~~Height/setback plane for DADUs along side setbacks.~~



10.47.050 Single-family ~~dwelling~~s.

All single-family dwellings ~~units~~, where defined in WCC [10.08.055](#) and where allowed by Chapter [10.10](#) WCC, District Use Chart, shall meet the following standards when on a lot with a total of five or more dwelling units, unless otherwise regulated within this code:

(1) Purpose.

- (a) To enhance the character of the street and neighborhood.
- (b) To maintain “eyes on the street” for safety to pedestrians and to create a more welcoming and interesting streetscape.
- (c) To deemphasize garages and driveways as major visual elements along the street.
- (d) To provide usable open space for residents.

(2) Standards.

(a) Entry Standards. All new dwellings shall provide a covered pedestrian entry with minimum weather protection of four feet by four feet (a covered porch or recessed entry).

~~(i) Clear and obvious pedestrian access between the sidewalk (or the street if there is no sidewalk) and the building entry is required for new dwellings. The driveway may be used for pedestrian access.~~

~~(ii) All new dwellings shall provide a covered pedestrian entry with minimum weather protection of four feet by four feet (a covered porch or recessed entry).~~

~~(iii) See the street setback exception for roofed porches and covered entries in WCC 10.46.080(2)(a)(iii).~~

~~(b) Driveway Standards.~~

~~(i) Minimum and maximum driveway widths for single family houses facing a public street as measured at the property/right of way line are set forth in Table 10.47.050(2)(b) below. Paved areas for vehicle storage or other uses may be wider if they are no less than five feet from the street property/right of way line.~~

~~Table 10.47.050(2)(b)~~

~~Driveway width measured at the property/right of way line.~~

Use Type	Minimum Width	Maximum Width
Residential	-	-
- Lot width = 50 feet or greater	10 feet	18 feet
- Lot width < 50 feet	10 feet	12 feet or 18 feet if shared with adjacent lot

~~(c) Garage and Carport Setbacks. Garages and carports are subject to the setback standards in Chapter 10.46 WCC.~~

~~(b) Garage Standards. Garages (attached or detached) may occupy no more than 50 percent of the width of the ground-level facade facing the street. Garage doors may exceed this limit up to a maximum of 65 percent of the width of the ground-level facade facing the street provided at least two of the following design details are utilized:~~

~~(i) A decorative trellis over at least the entire width of the garage door(s).~~

~~(ii) A window or windows are placed above the garage on a second story or attic wall.~~

~~(iii) A balcony that extends out over the garage and includes decorative support columns.~~




~~(iv) Utilizing all single vehicle car doors as an alternative to wider garage doors suitable for two-car garages.~~

~~(v) Windows on the garage door.~~

(vi) Decorative details on the garage door. Standard squares on a garage door will not qualify as a decorative detail.

Figure 10.47.050(2)(d)

Garage design detail examples.

		
<p>A decorative trellis over the garage door.</p>	<p>A balcony over the garage, single doors (instead of a wider double door), and windows above the garage on a second story.</p>	<p>A balcony over garage with decorative support columns.</p>

(ce) Standards for Minimum Usable Open Space.

(i) All new single-family dwelling units shall provide a contiguous open space to the side or rear of the dwelling with a minimum dimension of 15 feet on all sides. This space may not be used for vehicle or other large object storage but be available for human activity. The open space may be in the street setback if facing a private lane.

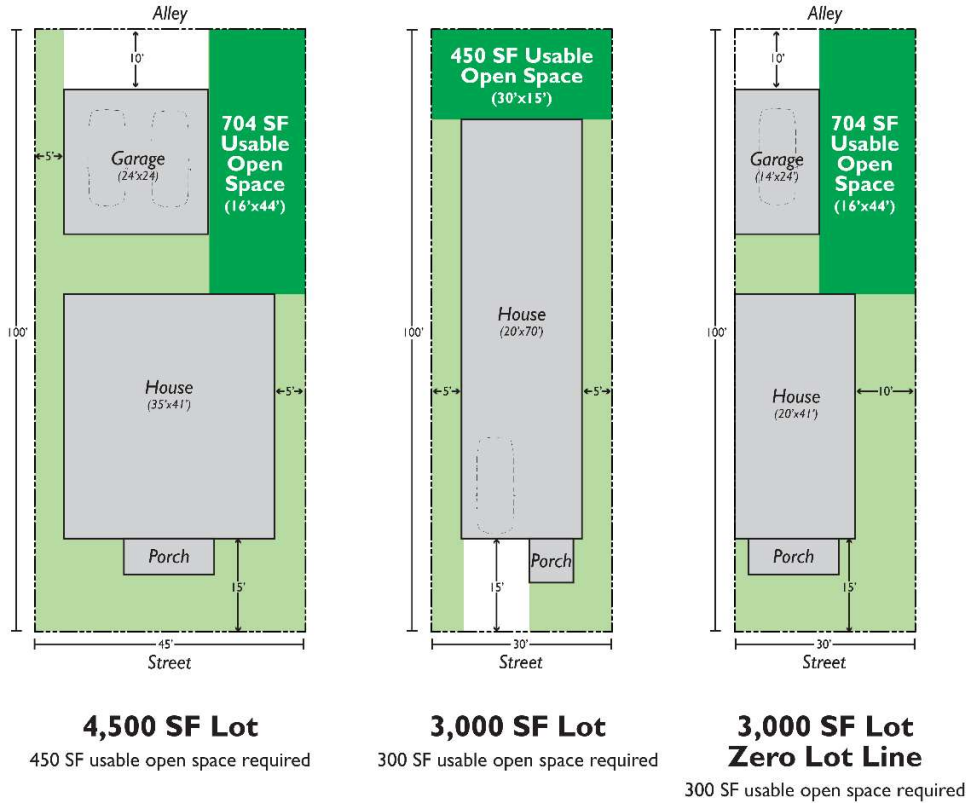
(ii) The open space(s) shall be equivalent to 10 percent (minimum) of the lot area. For example, a 4,000-square-foot lot would require a contiguous open space of at least 400 square feet, or 20 feet by 20 feet in area. Figure 10.47.050(2)(e) illustrates some configurations that comply for residences with alley access.

(iii) Drive aisles shall not count in the calculations for usable open space.

(iv) Additions shall not create or increase any nonconformity with this standard.

Figure 10.47.050(2)(e)

Examples of how to provide the minimum amount of usable open space.



~~(f) Multiple Single Family Dwellings on One Lot. Multiple single family dwellings may be placed on one lot provided the dwellings are located and designed in a manner so that the lot could be subdivided in the future and meet applicable lot dimensions, setbacks, and lot coverage standards applicable to the zoning district and other applicable standards in this title and WCC Title 11. The burden of proof for demonstrating compliance with the applicable standards for the potential short subdivision shall be on the applicant. Submittal requirements will therefore exceed those for a standard building permit. Additional information includes, but may not be limited to, a site plan that includes the "proposed" lot lines, access and utilities, any applicable easements, dimensions, and any other applicable information necessary to demonstrate compliance. Permitting multiple single family dwellings on a single lot in no way guarantees the ability to subdivide in the future. Future proposed land divisions would need to meet the standards in place at the time of application.~~

10.47.060 Single-family – Cluster.

(2) As an alternative to the cluster provisions in subsection (1) of this section, lots may be exempted from minimum lot area, depth, width, and rear setback standards, provided they are arranged in clusters of four to 12 dwelling units around a centralized common open space and meet the following standards:

(j) Driveway, Access, Parking, and Garages.

~~(i) Dwellings adjacent to a street are subject to applicable driveway and garage standards for single-family dwellings in WCC [10.47.050](#).~~

(ii) Parking and garages for individual dwellings may be configured separately on each lot or dwelling site (attached or detached) or clustered in shared parking areas.

(iii) Parking areas and garages (individual or shared) shall not be sited adjacent to common open spaces. The director may allow exceptions where site and/or building design features are integrated to minimize the impacts of garages and parking areas on the function and character of the common open space.

(iiiiv) Where parking areas and garages are provided on each dwelling lot or site, shared private lanes may be used to access up to six dwelling units. Shared private lanes are subject to the private lane standards in WCC [11.20.020](#), except:

(A) Maximum length: 150 feet.

(B) Minimum width: 15 feet paved lane within a 20-foot-wide easement.

~~10.47.070 Single-family – Courtyard.~~

~~10.47.080 Infill-cottage housing (two to three units).~~

10.47.090 Cottage housing (four or more units).

Cottage ~~housing~~dwelling units, where defined in WCC ~~10.08.050~~ [10.08.055](#) and where allowed by Chapter [10.10](#) WCC, District Use Chart, are subject to compliance with the Residential Design Guidelines and shall meet the following standards when on a lot with a total of five or more dwelling units, when four or more units are proposed unless otherwise regulated within this code:

(1) Purpose.

(a) To provide housing types that are responsive to changing household demographics (e.g., retirees, small families, young professionals).

(b) To provide opportunities for more affordable housing within single-family neighborhoods.

(c) To encourage creation of functional usable open space in residential communities.

(d) To promote neighborhood interaction and safety through design.

(e) To ensure compatibility with neighboring uses.

(f) To provide opportunities for infill development consistent with goals of the Wenatchee urban area comprehensive plan.

Figure 10.47.090(1)(a)

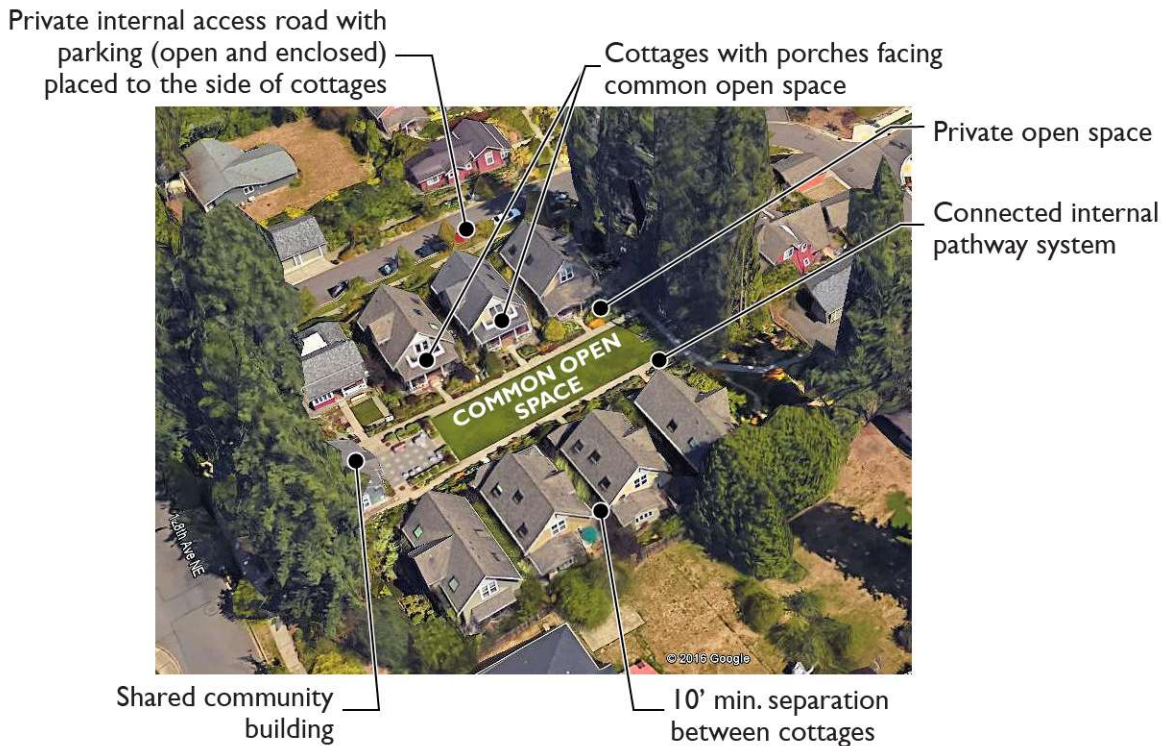
Cottage housing development examples.



Left example: Note the transition between the common open space on the left and the cottages includes a semi-private garden space and a porch. The middle and right images illustrate the standard “commons” as a simple green space for informal recreation activities. In all examples, the units orient towards the open space and include porches and a small semi-private garden.

Figure 10.47.090(1)(b)

Cottage housing site plan example.



(2) Standards.

(a) Cottage ~~housing development~~ options include ownership, rental, condominium, and subdivision.

(b) Subdivision. Where ~~a cottages housing development~~ involves a subdivision, development parameters as determined by the city shall be placed on the final plat or final unit lot subdivision.

(c) Minimum and Maximum Number of Cottages.

(i) A single cottage ~~housing~~ development shall include a minimum of ~~four~~ five dwelling units.

(ii) ~~Four-Five~~ to 12 cottage structures may make up a cluster (a grouping of cottages situated around a common open space or pedestrian circulation network, each with an entry facing the courtyard or pedestrian system).

(d) Maximum Cottage Size. The total floor area for a cottage shall be no greater than 1,200 square feet; provided, the ground floor is not greater than 800 square feet.

(e) Density. For the purposes of complying with the maximum density of the applicable zoning district, each cottage shall be counted as one-half dwelling unit. For example, for an existing lot that qualifies for three dwelling units would qualify for six cottages.

(f) Setback Exceptions to Chapter 10.46 WCCs.

~~(i) Cottages built along streets are subject to the applicable street setbacks in Chapter 10.46 WCC.~~

~~(ii) Cottages built adjacent to side property lines are subject to the applicable side setback.~~

(iii) Cottages built along rear property lines of the parent lot(s) shall maintain a 10-foot minimum setback in the RS and RL zones and a five-foot minimum setback in all other applicable zones.

(iv) The minimum internal setback between cottages is 10 feet, except that two cottages may share a common wall as in a duplex configuration. Within a given development, up to one-half of the cottages may feature the shared wall/duplex configuration.

(g) Site Design and Orientation. Dwellings within a cottage ~~housing~~ development shall be oriented to promote a sense of community, both within the development and, with respect to the larger community, outside of the cottage project. A cottage development shall not be designed to “turn its back” on the surrounding neighborhood by locating rear yards to face a public street.

~~(i) Clear and obvious pedestrian access between the sidewalk (or the street if there is no sidewalk) and the building entry is required for new cottage dwellings.~~

~~(ii) All new cottage dwellings adjacent to a street shall meet the Residential Design Guidelines, especially Sections C.1 (Ground related units facing streets, common pathways, or common open space), C.2.1 (Pedestrian connectivity within multi-unit residential development with more than one building) and E.5.1 (Building entrances: configuration and details).~~

~~(iii) Common open space shall be provided in a centrally located focal area for the development and abut at least 50 percent of the cottages in the development (see open space standards below); direct access to the common open space shall be provided for each cottage by a paved walkway. See also Residential Design Guideline D.3, Residential Open Space Elements.~~

(iv) One existing single-family dwelling may be incorporated into a cottage housing development of six or more cottages as a residence. Nonconforming residences with respect to architectural standards shall be permitted.

(iiv) A cottage housing development may contain one community building that is clearly incidental in use and similar in size and architectural design of cottages. Such community building shall be located on the same site as the cottage housing development and be commonly owned by the residents.

(iiiv) Cul-de-sacs are not permitted within a cottage development, unless it can be demonstrated that no other configuration is feasible.

~~(h) Building Design and Orientation. See Section E in the Residential Design Guidelines.~~

(iv) Open Space.

~~(A) A common open space is required for each cottage cluster. Common open space shall be provided in a centrally located focal area for the development and abut at least 50 percent of the cottages in the development (see open space standards below); direct access to the common open space shall be provided for each cottage by a paved walkway. See also Residential Design Guideline D.3, Residential Open Space Elements.~~

~~(B) A minimum of 200 square feet of common open space shall be provided per dwelling unit.~~

~~(1A) Common open space within a development shall be a minimum of 1,000 square feet in size with no dimension less than 15 feet, regardless of the number of dwelling units.~~

~~(2B) Common open space shall meet the requirements of the Residential Design Guidelines, especially Section C.3.2.b, and be improved for passive or active recreational use. Examples may include, but are not limited to, courtyards, noncommercial orchards, landscaped picnic areas or gardens, wildlife viewing areas, etc.~~

~~(3G)~~ Fences are not allowed within any common open space area.

~~(4D)~~ Common open space shall be located in a separate tract or tracts and owned in common by all property owners.

See Figures 10.47.090(1)(a) and (b) for examples.

~~(Ciii)~~ Each cottage unit shall include a minimum of 200 square feet of private open space (in addition to common open space). The private open space shall have no dimension less than 10 feet, be located adjacent to the unit which it serves and feature direct access to the private open space via door. Examples include a semi-private garden space between a dwelling and the common open space. See Figures 10.47.090(1)(a) and (b) for examples.

~~(Div)~~ Surface water management facilities may be included in the open space calculation for meeting minimum requirements, if it can be demonstrated that they are designed so that residents can use the space similar to other open space designations.

~~(hj) Access, Driveways, Parking, and Garages.~~

~~(i) Unless a higher classification of road is required per public works preapproved plans or by the city engineer to provide for consistency with the Wenatchee urban area plan, internal access and circulation shall meet the requirements of, Fire Apparatus Access Road, of the most current edition of the International Fire Code, as amended. Where a standard residential subdivision or binding site plan is proposed associated with these developments, the standards provided in WCC 11.20.020, Streets, shall also apply.~~

~~(ii) Each cottage dwelling unit shall provide at least one off-street parking space per bedroom, up to two parking spaces per unit.~~

~~(iii) Cottages adjacent to a street are subject to applicable driveway and garage standards for single-family dwellings in WCC 10.47.050.~~

~~(iv) Parking areas shall be located to the side or rear of cottage clusters (not between the cottages and the street front unless no other configuration is feasible, as determined by the director). Landscaping in form of hedges or architectural barriers is required to screen surface parking lots for neighboring properties and common open spaces.~~

~~(iv)~~ Private garages are allowed to be attached to or detached from individual cottages if less than 250 square feet in size and accessible by an alley or shared access. Private garages shall match the architectural style of the units and meet the same separation requirements as adjoining units.

~~(iiv)~~ Enclosed parking areas may be grouped into a single structure. Individual garage doors shall be one car width.

~~(k) Landscaping. Cottage housing developments shall adhere to the landscape and screening chapter, Chapter 10.62 WCC. See also Residential Design Guidelines Section D.1.~~

~~(l) Architectural Design. See Residential Design Guidelines Section E: Building Design.~~

~~(im)~~ Covenants enforced by a homeowner's association shall be required to include the perpetual maintenance of all common areas and open spaces and preclude the conversion to another use.

10.47.100 Duplexes.

All duplexes, where defined in WCC [10.08.055](#) and where allowed by Chapter [10.10](#) WCC, District Use Chart, shall meet the following standards when on a lot with a total of five or more dwelling units, unless otherwise regulated within this code:

(1) Purpose. The intent of these standards is to allow for residential infill that is compatible with the character and scale of existing residential neighborhoods without adversely impacting neighbors, the broader neighborhood, and the community as a whole. Furthermore, the purpose of these duplex design standards is to:

(a) To enhance the character of the street and neighborhood.

(b) To maintain "eyes on the street" for safety to pedestrians and to create a more welcoming and interesting streetscape.

(c) To deemphasize garages and driveways as major visual elements along the street.

(d) To provide usable open space for residents.

(2) Standards.

~~(a) Off-Street Parking. Each dwelling unit shall have one parking stall per bedroom up to two stalls per dwelling unit.~~

~~(b) Access. Access to principal or minor arterial streets shall be limited to a single driveway (18 feet maximum width) per unit unless accessed from an alley or as otherwise approved by the city engineer.~~

~~(ae)~~ Architectural Design. See Residential Design Guidelines Section E: Building Design.

~~(b3)~~ Minimum Usable Open Space. Duplexes shall include usable open space for each dwelling unit:

(ia) Each unit shall include a contiguous open space with a minimum dimension of 15 feet on all sides. The director will allow a reduced minimum dimension of 10 feet on all sides provided multiple usable open spaces are integrated such as upper level decks or balconies.

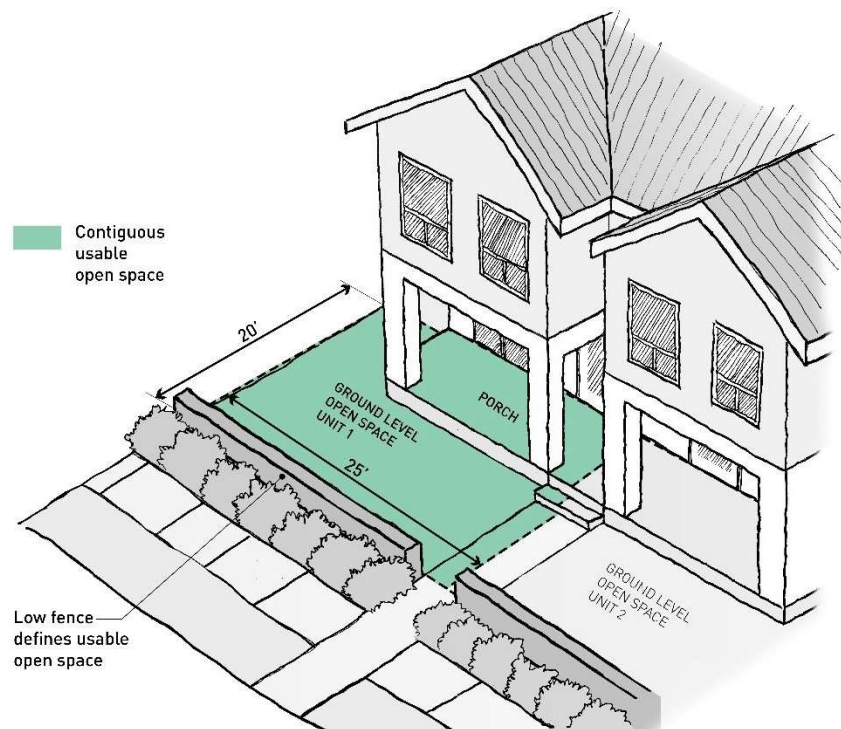
(iib) The combined open spaces shall be equivalent to 10 percent (minimum) of the duplex lot area. For example, an 8,000-square-foot lot would require a usable open spaces of at least 800 square feet (e.g., 20 feet by 20 feet (400 square feet) for each unit). Unenclosed ground-level porches and patio areas and upper floor decks may be included in the open space calculations.

(iiie) Street setbacks may be used to meet this standard provided they are defined with a fence meeting the standards of WCC [10.48.130](#).

(ivd) Drive aisles shall not count in the calculations for usable open space.

(ve) Additions shall not create or increase any nonconformity with this standard.

Figure 10.47.100(a)
Example of integrating a duplex's minimum usable open space into the street setback.



~~10.47.110 — Patio homes.~~

10.47.120 Courtyard housing.

All courtyard ~~housing~~ developments, where defined in WCC [10.08.0550](#) and where allowed by Chapter [10.10](#) WCC, District Use Chart, are subject to compliance with the Residential Design Guidelines and shall meet the following standards when on a lot with a total of five or more dwelling units, unless otherwise regulated within this code:

~~Courtyard housing is characterized by attached multifamily units arranged around a courtyard. The units may be all ground related, like townhouses, or a combination of ground related and~~

~~upper story residences. Courtyard housing on a single lot is a special case of “multifamily development,” but standards for courtyard housing are separated here because of that housing type’s unique configuration around a courtyard or open space.~~

(1) Purpose.

- (a) To provide housing types that are responsive to changing household demographics (e.g., retirees, small families, young professionals).
- (b) To provide opportunities for more affordable housing within single-family neighborhoods.
- (c) To encourage creation of functional usable open space in a pleasant setting.
- (d) To promote neighborhood interaction and safety through design.
- (e) To ensure compatibility with neighboring uses.
- (f) To provide opportunities for infill development consistent with goals of the Wenatchee urban area comprehensive plan.

Figure 10.47.120
Courtyard housing development examples.



(2) Standards.

(a) Courtyard ~~housing~~-development options include ownership, rental, condominium, and subdivision.

(b) Subdivision. Where a courtyard ~~housing~~-development involves a subdivision, development parameters, as determined by the city, may be placed on the final plat or final unit lot subdivision.

(c) Minimum and Maximum Number of Dwelling Units.

~~(i)~~ A single courtyard ~~housing~~-development shall include a minimum of ~~four~~ five dwelling units, up to a maximum of 24 units. The city may allow development of more than 24 dwelling units if all of the purposes stated above are achieved.

(d) Density. The maximum density shall be as described in WCC [10.46.020](#), except that dwelling units under 1,000 square feet (excluding unenclosed front porches) may be counted as three-quarters of a unit ~~(resulting in a one-third density bonus)~~.

(e) Setback Exceptions to Chapter 10.46 WCCs:

~~(i) Courtyard buildings facing public streets are subject to the applicable street setbacks in Chapter 10.46 WCC.~~

~~(ii) Courtyard buildings built along the side yards of adjacent properties (not in the development) are subject to the applicable side setback.~~

(iii) Courtyards built along rear property lines of adjacent properties (not in the development) shall maintain 10-foot minimum setbacks in the RS and RL zones and five-foot minimum setbacks in all other applicable zones.

~~(iv)~~ There is no minimum internal setback between units within a courtyard development.

(f) Site Design and Orientation. Dwellings within a courtyard ~~housing~~-development shall be oriented to promote a sense of community, both within the development, and with respect to the larger community, outside of the courtyard project.

~~(i) Clear and obvious pedestrian access and visual access between the sidewalk (or the street if there is no sidewalk) and the courtyard is required for new courtyard dwellings.~~

~~(ii) All new courtyard dwellings adjacent to a street shall meet the Residential Design Guidelines, especially Sections C.1 (Ground related units facing streets, common pathways, or common open space), C.2.1 (Pedestrian connectivity within multi-unit residential development with more than one building) and E.5.1 (Building entrances: configuration and details).~~

~~(iii) Common open space shall be provided in a centrally located focal area for the development and abut at least 50 percent of the courtyards in the development (see open space standards below); direct access to the common open space shall be provided for each courtyard by a paved walkway.~~

~~(g) Building Design and Orientation. See Section E in the Residential Design Guidelines.~~

~~(gh) Open Space.~~

(i) A common open space is required for each courtyard cluster. Common open space shall be provided in a centrally located focal area for the development and abut at least 50 percent of the courtyards in the development (see open space standards below); direct access to the common open space shall be provided for each courtyard by a paved walkway.

(ii) A minimum of 200 square feet of common open space shall be provided per dwelling unit.

(A) Common open space within a development shall be a minimum of 800 square feet in size with no dimension less than 15 feet, regardless of the number of dwelling units.

(B) Common open space shall meet the requirements of the Residential Design Guidelines, especially Section C.3.2.b, and be improved for passive or active recreational use. Examples may include, but are not limited to, courtyards, orchards, landscaped picnic areas or gardens.

(C) Fences are not allowed within any common open space area.

(D) Common open space shall be located in a separate tract or tracts and owned in common by all property owners.

(E) Setbacks, porches and other measures required in Residential Design Guideline C.1.1 shall not be counted as common open space.

(iii) Surface water management facilities may be included in the open space calculation for meeting minimum requirements, if it can be demonstrated that they are designed so that residents can use the space similar to other open space designations.

~~(hi) Access, Parking, and Garages.~~

~~(i) Unless a higher classification of road is required per public works preapproved plans or by the city engineer to provide for consistency with the Wenatchee urban area plan, internal access and circulation shall meet the requirements of, Fire Apparatus Access Road, of the most current edition of the International Fire Code, as amended. Where a standard residential subdivision or binding site plan is proposed associated with these developments, the standards provided in WCC 11.20.020, Streets, shall also apply.~~

~~(ii) Each courtyard dwelling unit shall provide one off street parking space per bedroom, up to two parking spaces per unit.~~

~~(iii) Parking areas shall be located to the side or rear of courtyard units (not between the courtyard and the street front unless no other configuration is feasible, as determined by the city). Landscaping in the form of hedges or architectural barriers is required to screen surface parking lots from neighboring properties and common open spaces.~~

(iv) Private garages are allowed to be attached to or detached from individual courtyard units if less than 250 square feet in size and accessible by an alley or shared private lane. Private garages shall match the architectural style of the courtyard units.

(iiv) Enclosed parking areas may be grouped together within one or more structures. Individual garage doors shall be one car width.

~~(j) Landscaping. Courtyard housing developments shall adhere to the landscape and screening chapter, Chapter 10.62 WCC. See also Residential Design Guidelines Section D.1.~~

(ik) Covenants enforced by a homeowner's association shall be required to include the perpetual maintenance of all common areas and open spaces and preclude the conversion to another use.

10.47.130 Townhouses.

All townhouses, as defined in WCC ~~10.08.135-10.08.055~~ and where allowed by Chapter 10.10 WCC, District Use Chart, are subject to compliance with the Residential Design Guidelines and shall meet the following standards when on a lot with a total of five or more dwelling units, unless otherwise regulated within this code:

(1) Purpose.

(a) To provide housing types that are responsive to changing household demographics (e.g., retirees, small families, young professionals).

(b) To encourage creation of functional usable open space in residential communities.

(c) To enhance the streetscape and promote neighborhood interaction and safety through design.

(d) To ensure compatibility with neighboring uses.

(e) To enhance design character and livability of townhouse developments.

(f) To provide opportunities for infill development consistent with goals of the Wenatchee urban area comprehensive plan.

(2) Standards.

~~(a) Setback Exceptions to Chapter 10.46 WCC.s. The minimum setbacks set forth in Chapter 10.46 WCC apply to development frontage and external side and rear setbacks of the entire townhouse development. Townhouse units within a single development are exempt from internal side and rear setbacks unless otherwise noted below. However, if the development includes the creation of new lots, the new development shall conform to the provisions of WCC 11.32.080, Unit lot subdivisions.~~

~~(i) Townhouses built along and oriented to public streets are subject to the applicable street setbacks in Chapter 10.46 WCC. Additionally, the setbacks for garages accessing a public street apply.~~

~~(ii) Townhouses built along side property lines of adjacent properties are subject to the applicable side setbacks in Chapter 10.46 WCC.~~

(iii) Townhouses built along rear property lines of adjacent properties shall maintain 10-foot minimum setbacks in the RS and RL zones and five-foot minimum setbacks in all other applicable zones.

(iv) The minimum internal setback between townhouse buildings built around and facing a pathway is 15 feet between the facades of each building.

~~(v) The minimum setback for townhouses (including porches and covered entries) not fronting on a public street shall be:~~

~~(A) Ten feet from the back edge of the sidewalk or 15 feet from the edge of roadway/driveway pavement for an internal drive (pursuant to subsection (2)(d) of this section).~~

~~(B) Fifteen feet from a private lane easement or tract.~~

~~(C) The minimum setback for a garage facing a private lane or internal drive aisle shall be at least five feet further from the building facade facing the private lane or internal drive aisle, including a porch or covered entry, and no less than 20 feet from the edge of roadway/driveway pavement and sidewalk if applicable.~~

~~(vi) See also requirements for ground related residential units in Residential Design Guidelines Section C.1.1.~~

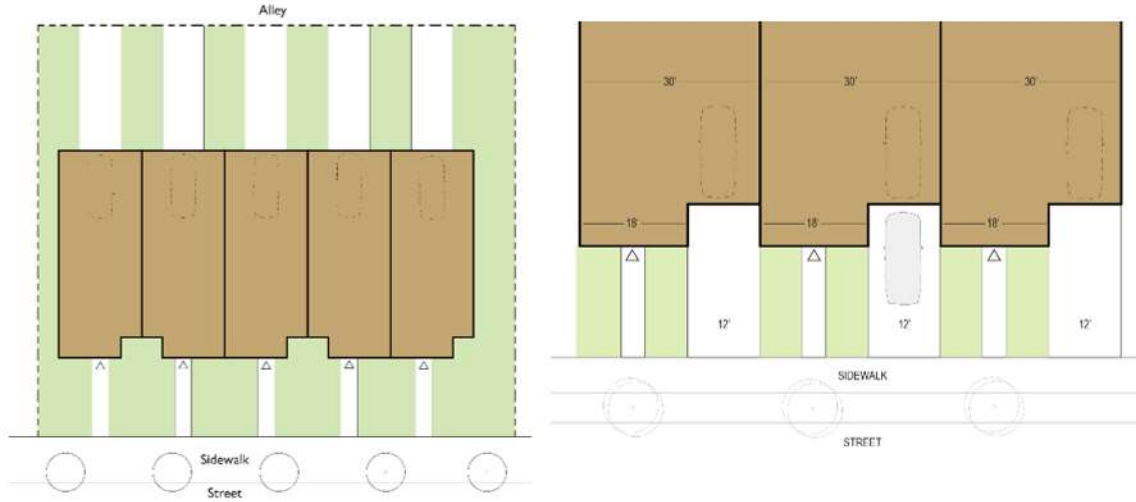
~~(b) Entries. See Residential Design Guidelines Section E.5.1.~~

~~(c) Front Loaded Townhouses. Private driveways (maximum 12 feet wide) and garages facing the street (single car width and a maximum 10 feet wide) are allowed in the following circumstances:~~

~~(i) Where townhouse units are at least 30 feet wide. See Figure 10.47.130(2)(c) below for examples; and~~

~~(ii) Where townhouses front a street other than a designated principal or minor arterial.~~

Figure 10.47.130(2)(c)
Townhouse garage access configurations – acceptable and unacceptable examples.



The left example includes alley access to private garages. The right example includes front-loaded townhouses units that meet the 30' minimum width standard for such designs that include individual 12' maximum driveway widths accessed from the street.



The left example features individual driveways and garages for townhouses less than 30' wide (and thus would not be allowed).

~~(d) Townhouses with Shared Access.~~

~~(i) Unless a higher classification of road is required per public works preapproved plans or by the city engineer to provide for consistency with the Wenatchee urban area plan, internal access and circulation within a townhouse development shall meet the requirements of, Fire Apparatus Access Road, of the most current edition of the International Fire Code, as amended. Where a standard residential subdivision or binding site plan is proposed associated with these developments, the standards provided in WCC 11.20.020, Streets, shall also apply.~~

~~(ii) Minimum building separation across internal drive aisles shall be 24 feet. Projections into this minimum building separation standard are permitted for each building consistent with the reduced side setback provisions referenced in WCC 10.46.080(4). The purpose is to provide adequate vehicular turning radius, allow for landscaping elements on at least one side, and to provide adequate light and air on both sides of the dwelling units and vehicle areas, which often function as usable open space for residents.~~

~~(iii) Portions of internal drive aisles within townhouse developments that have parking spaces that are directly adjacent shall meet the dimensional standards in WCC 10.60.030.~~

~~(e) Parking. Off street parking standards for townhouses are set forth in WCC 10.60.080 as a type of multifamily dwelling (based on the number of bedrooms).~~

(bf) Usable Open Space. Townhouse dwellings shall provide open space at least equal to 10 percent of the gross floor area. The required open space may be provided by one or more of the following:

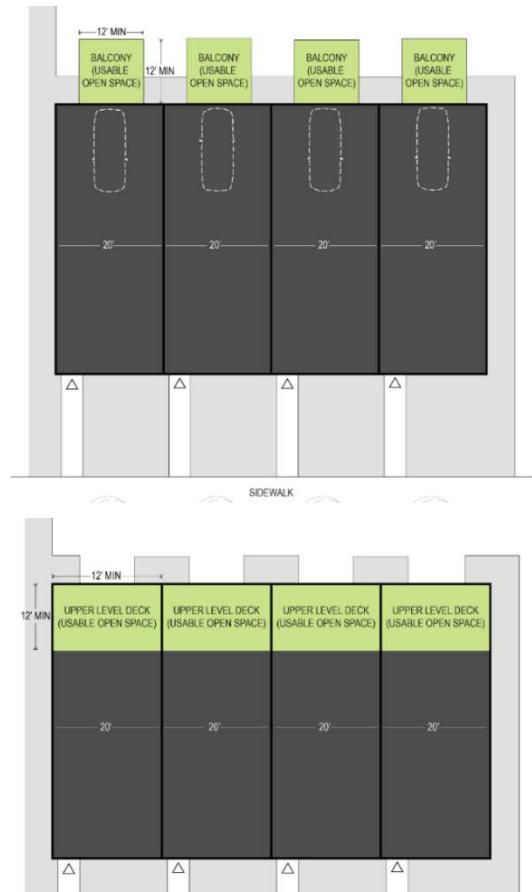
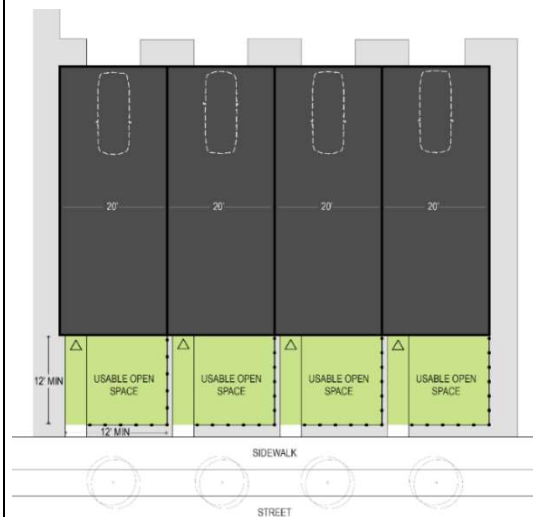
- (i) Private ground-level open space that is directly adjacent and accessible to dwelling units. Such space shall have minimum dimensions of at least 12 feet on all sides and be configured to accommodate human activity such as outdoor eating, gardening, toddler play, etc. Street setbacks from private lanes or drives may be used to meet this standard, provided they are defined with a fence meeting standards of WCC [10.48.130](#).
- (ii) Balconies, roof decks or porches.

Figure 10.47.130(f)(1)

Examples of usable open space adjacent to townhouse units.



The lower left example includes usable open space in the front yard. The lower right example includes an upper level deck the full width of the unit. The right example uses a balcony that's placed over the garage driveway.



~~(g) Building Design. See Residential Design Guidelines Section E.~~

10.47.150 Multifamily ~~development~~.

All multifamily dwellings, as defined in WCC [10.08.055](#), and where allowed by Chapter [10.10](#) WCC, District Use Chart, are subject to compliance with the Residential Design Guidelines and shall meet the following standards when on a lot with a total of five or more dwelling units, unless otherwise regulated within this code. Exception: Courtyard housing (see WCC [10.47.120](#)) and townhouses (see WCC [10.47.130](#)) are exempt from the standards herein.

(1) Purpose.

(a) To provide housing types that are responsive to changing household demographics (e.g., retirees, small families, young professionals).

(b) To provide opportunities for more affordable housing close to transit, employment and services.

(c) To ensure compatibility with neighboring uses.

(d) To provide opportunities for infill development consistent with goals of the Wenatchee urban area comprehensive plan.

Figure 10.47.150(1)
Multifamily ~~housing~~ development examples.



(2) Standards.

(a) Multifamily housing development options include ownership, rental, and condominium.

~~(b) Density. The maximum density shall be as described in WCC 10.46.020.~~

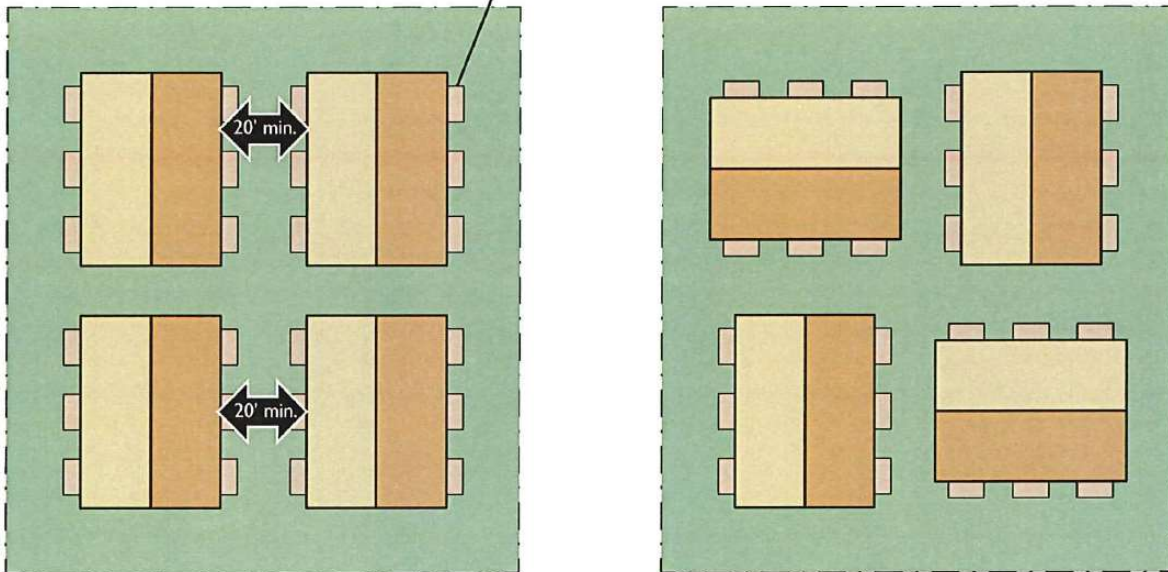
~~(b) Setback Exceptions to Chapter 10.46 WCCs.~~

~~(i) Multifamily buildings are subject to the applicable setbacks in Chapter 10.46 WCC.~~

(ii) Multifamily development built along rear property lines of adjacent properties (not in the development) shall maintain 10-foot minimum setbacks in the RS and RL zones and five-foot minimum setbacks in all other applicable zones unless a smaller minimum is established in this title.

(iii) If more than one multifamily building is built on a single lot, building faces with windows facing into opposing/adjacent residential units shall be at least 20 feet apart.

Figure 10.47.150(2)
 Multifamily ~~housing~~ development examples.
 Balcony and window



The buildings in the left example, where units and balconies face each other, shall be placed at least 20 feet apart. The configuration of buildings on the right, where units face the side of other buildings and thus feature less privacy impacts, are not subject to the 20-foot setback requirement.

~~(d) Site Design and Orientation. Dwellings within a multifamily housing development shall be oriented to provide both access and privacy for the residents.~~

~~(i) Clear and obvious pedestrian access and visual access between the sidewalk (or the street if there is no sidewalk) and any shared entrance(s) or exterior primary entrances to a unit (if applicable) are required for new multifamily dwellings.~~

~~(ii) For all new multifamily dwellings adjacent to a street, see Residential Design Guidelines, especially Sections C.1, C.2.1 and E.5.1.~~

~~(e) Building Design and Orientation. See Section E in the Residential Design Guidelines.~~

(cf) Open Space.

(i) Amount of Required Residential Open Space. All new multifamily uses shall either provide on-site open space of 100 square feet per dwelling unit or 10 percent of residential unit floor area (excluding hallways and common areas) that provides functional leisure or recreational activities. (The lesser amount of the two calculations is acceptable.) The open space may be in the form of common open space, balconies, roof decks, indoor recreation spaces or similar features provided they meet the requirements of the Residential Design Guidelines

Section C.3. Required setback areas shall count as part of the required open space requirement if they meet the requirements in the Residential Design Guidelines.

(ii) Surface water management facilities may be included in the open space calculation for meeting minimum requirements, if it can be demonstrated that they are designed so that residents can use the space similar to other open space designations.

~~(dg) Access, Parking, and~~ Garages.

~~(i) Unless a higher classification of road is required per public works preapproved plans or by the city engineer to provide for consistency with the Wenatchee urban area plan, internal access and circulation shall meet the requirements of Fire Apparatus Access Road, of the most current edition of the International Fire Code, as amended. Where a standard residential subdivision or binding site plan is proposed associated with these developments, the standards provided in WCC 11.20.020, Streets, shall also apply.~~

~~(ii) Each multifamily dwelling unit shall provide at least one off-street parking space per bedroom, up to two parking spaces per unit.~~

~~(iii) Parking areas shall be located to the side or rear of multifamily units (not between the multifamily building and the street front unless no other configuration is feasible, as determined by the director). Landscaping in the form of hedges or architectural barriers is required to screen surface parking lots for neighboring properties and common open spaces.~~

~~(iv) Private garages are allowed to be attached to or detached from individual multifamily units if less than 250 square feet in size and accessible by an alley or shared driveway. Private garages shall match the architectural style of the multifamily units.~~

~~(iiv) Enclosed parking areas are encouraged to be clustered, with no more than three parking spaces together within one structure not greater than 800 square feet. Individual garage doors shall be one car width.~~

~~(h) Landscaping. Multifamily housing developments shall adhere to the landscape and screening chapter, Chapter 10.62 WCC. See also Residential Design Guidelines Section D.1.~~

~~(i) Architectural Design. Conform to the Residential Design Guidelines Section E.~~

~~(ej) General Storage. Storage of personal property and materials, of residents as well as managers and owners of a development, shall be located outside of required street setback areas, and it shall be entirely within an enclosed building or screened from view of the surrounding properties with a sight-obscuring fence and landscaping. Developments for which WCC 10.48.320 applies shall also meet those standards.~~

(f) Bicycle facilities. In multi-story multifamily buildings where some units are not ground related, provide at least one secure bicycle storage space for each unit. The storage space may be within a structured parking area, special structure or within the dwelling unit, if approved by the Director.

~~10.47.170 Alley access and improvements.~~

~~(1) Alley access is required for all residential development on sites adjacent to alleys where the city engineer determines that the alley is sufficiently improved to provide adequate circulation.~~

~~(2) Developments requiring six or more parking spaces accessing an alley shall pave the alley if needed and make necessary stormwater improvements.~~

Chapter 10.48 GENERAL DEVELOPMENT REGULATIONS

10.48.020 Driveways and Access.

The following regulations shall apply to all zoning districts and overlays:

(1) All driveway widths, setbacks, and separation distances are measured at the property/right-of-way line.

~~(2) Access required. Every lot shall be provided with and maintain satisfactory access by a driveway from a public or private street or access easement. The city determines whether access will be by right of way, vehicular easement or tract on a case by case basis consistent with the provisions of city code and the comprehensive plan. Lots of record which have no recognized public or private street frontage shall demonstrate legal access prior to the issuance of any development permits for said lot. Provisions for new lots served by private lanes or shared driveways are set forth in WCC 11.20.020 except as modified by this title as applicable.~~

(3) Number of driveways. Only one driveway per lot may be permitted for a street frontage which does not exceed eighty feet. Two driveways may be permitted for frontages exceeding eighty feet. All driveways in excess of the number allowed shall be approved if the City Engineer determines that said driveway would be of benefit to the City's traffic circulation.

(4) Location.

(a) All driveways shall be set back a minimum of five feet from a side property line of an adjacent property.

(b) Driveways shall be a minimum of 30 feet from another driveway on the same lot or within the same development.

~~(c) Egress and ingress points Driveways along all locally classified principal arterials, minor arterials, and collectors shall:~~

(ia) Be no closer than 100 feet from another access within the same development project.

(iib) When feasible, be no closer than 150 feet from a minor arterial intersection or 250 feet from a principal arterial intersection.

(d4) On a ~~corner~~ lot with more than one street frontage, access shall be from the lower classified street as depicted in the circulation map of the Wenatchee urban area comprehensive plan. Accessing from the higher classified street shall only be allowed if the city engineer determines that said access would be of benefit to the city's traffic circulation.

(e) Vehicular access shall be taken exclusively from the alley for all residential development on sites adjacent to alleys where the city engineer determines that the alley is sufficient to provide adequate circulation. Developments requiring six or more parking spaces accessing an alley shall make necessary improvements to the alley in accordance with the most current Public Works Standard plans and policies.

(53) Driveway widths. Maximum driveway widths on a public or private street as measured at the property/right-of-way line are set forth below:

(a) Driveway widths shall be a maximum of 18 feet for lots with one to four dwelling units and a maximum of 25 feet for lots with residential development of greater than four dwelling units. The maximum driveway width may be increased on the property at a distance of five feet from the back of sidewalk (or the planned sidewalk location based on current public works standards if the sidewalk is not yet constructed). at the curb cut shall be 24 to 30 feet for non-single family residential development and a maximum of 14 to 24 feet for residential development unless otherwise noted for specific housing types in Chapter 10.47 WCC.

(b) Driveway widths for commercial development shall be as determined by the city engineer.

10.48.040 Pedestrian connections.

All pedestrian connections shall meet the following standards unless otherwise regulated within this code:

(1) Pedestrian access to primary building entrances from adjacent public streets shall be provided. For ADU's and lots with four or less dwelling units, the driveway may be used for pedestrian access.

(2) An internal sidewalk or pathway system, at least five feet in width, that enhances pedestrian access within and through the site shall be provided, including from designated parking areas to primary building entrances. When pedestrian access is adjacent to driveways, they shall be differentiated by material or height. Single-family dwellings and duplexes are excluded from this requirement unless otherwise required in Chapter 10.47 WCC.

10.48.120 Vehicle parking.

~~All dwelling units in a residential zone~~~~All single family and single duplexes~~, where allowed by Chapter 10.10 WCC, District Use Chart, shall meet the following standards unless otherwise regulated within this code. For purposes of this section, recreational vehicles shall include, but not be limited to, motor homes, travel trailers, snowmobiles, motorcycles, jet skis and similar equipment. Agricultural equipment associated with a legal agricultural use on the site is exempt from the below restrictions.

Not more than a total of five cars, trucks, recreational vehicles, and trailers (per dwelling unit) may be parked outside ~~of a single family dwelling or duplex~~ on any lot in a residential zone. Said vehicles shall be parked ~~as provided in WCC 10.60.030(7) to the rear of the street setback line, except when stored in designated driveways. Driveways shall include a paved or graveled area accessible by curb cut, or on lots where no curb or curb cut or paving/gravel is installed, the driveway shall be considered a designated area equivalent to two parking spaces.~~

Chapter 10.60 OFF-STREET PARKING

10.60.030 General requirements.

(7) Except where otherwise provided by this title, off-street parking for new construction, including additions, must be within fully enclosed structures or located behind or to the side of buildings. Exceptions to this standard may be granted by the director, if site infeasibility can be demonstrated ~~or as provided in WCC 10.47. Single family residences and duplexes shall comply with WCC 10.48.120.~~

10.60.080 Off-street parking requirements.

The following requirements shall be met in all zoning districts, except when specifically regulated elsewhere in this code:

Land Use	Standards
Residential	
Accessory dwelling unit	Refer to WCC 10.47.040(2)(d)
Bed and breakfast, transient rental	1 space per bedroom

Land Use	Standards
Boarding or lodging room	1 space per bedroom
<u>Dwelling Unit (properties 6,000sf and less)</u>	<u>1 space per unit, except as in WCC 10.47.035</u>
<u>Dwelling Unit (properties above 6,000sf)</u>	<u>1 space per bedroom, not more than 2 spaces per dwelling unit, except as in WCC 10.47.035</u>
Duplex; patio homes	1 space per bedroom, but not more than 2 spaces per dwelling unit; refer to WCC 10.47.100(2)(a)
Supervised living facilities, welfare or correction institution, group homes (7 or more)	1 space per 5 beds
Home occupation	2 spaces per business
Senior housing	1 space per dwelling unit
Multifamily dwelling; townhouses	1 space per bedroom, but not more than 2 spaces per dwelling unit
Student housing	1.5 spaces per bedroom up to 3 bedrooms per unit. Above 3 bedrooms add

Land Use	Standards
	0.5 parking space per student.
Single-family dwelling, adult family home, group home (6 or less)	2 spaces per dwelling unit
Infill cottage housing (2 to 3 units)	1 space per bedroom, but not more than 2 spaces per dwelling unit; refer to WCC 10.47.080(2)(h)
Cottage housing (4+)	1 space per bedroom, but not more than 2 spaces per dwelling unit; refer to WCC 10.47.090(2)(j)
Courtyard housing	1 space per bedroom, but not more than 2 spaces per dwelling unit; refer to WCC 10.47.120(2)(i)
Live-work dwellings	Refer to WCC 10.47.140(2)(e)
Manufactured home community	Refer to WCC 10.47.180(2)(k)(i)

Chapter 10.65 CONDITIONAL USES

10.65.220 Public utilities and services.

(1) ~~Architectural Scale. New nonresidential buildings in the RF, RS, RL, RM, and RH zoning districts shall meet WCC 10.48.270Any buildings proposed shall be designed to harmonize with the residential structures of the neighborhood;~~

(2) Parking needs shall be evaluated by the hearing examiner consistent with Chapter 10.60 WCC, as amended~~All setback requirements of the district in which the public service facility is proposed shall be observed;~~

Title 15 Development Impact Fee

GENERAL PROVISIONS GOVERNING THE ASSESSMENT OF IMPACT FEES

15.02.040 Exemptions.

(5) An accessory dwelling unit ~~constructed on a single family lot.~~

Title 7 STREETS AND SIDEWALKS

SIDEWALK CONSTRUCTION STANDARDS

7.22.020 Sidewalks constructed during building improvement.

Curbs, gutters and sidewalks shall be constructed simultaneously with the improvement of real property on the street or streets abutting the property to be improved. "Improvement of the property" shall mean construction of a primary structure, or any remodels in any two-year period representing greater than 50 percent of the valuation of the structure as determined using the most recent ICC valuation and construction tables, or remodel adding 20 percent or more of gross floor area. The width of the sidewalk shall be determined by the city engineer after taking into consideration the width of existing sidewalks adjacent or contiguous to the property to be improved, the width of available right-of-way for the construction of sidewalks, and the zone in which the property is to be located; provided, however, that the minimum width of any sidewalk constructed pursuant to this chapter shall be four and one-half feet. In lieu of the above requirements, upon approval of the city engineer, the applicant shall dedicate right-of-way to the city of Wenatchee if adequate right-of-way is not available for required sidewalks. The amount of dedication required will be determined by the city engineer.

The development of Accessory Dwelling Units shall not require public street improvements.

City of Wenatchee
**Residential Design
Guidelines**



December 8, 2022

Table of Contents

A.	<i>Administrative</i>	3
A.1.	Purpose.....	3
A.2.	Administrative procedures.....	4
A.3.	Applicability.....	4
A.4.	Terms used in the guidelines.....	5
A.5.	Organization of the residential and mixed-use guidelines.....	5
B.	<i>Introduction</i>	6
B.1.	Residential development types.....	6
B.2.	Establishment of “Character Areas”.....	16
B.3.	Grandview Historic District.....	16
C.	<i>Site Planning</i>	18
C.1.	Relationship to street fronts and common pathways.....	18
C.2.	Pedestrian and non-motorized circulation.....	27
C.3.	Residential open space.....	28
C.4.	Vehicle access and parking.....	31
C.5.	Site planning for security.....	33
C.6.	Relationship to adjacent properties.....	35
C.7.	Service areas and mechanical equipment.....	36
D.	<i>Site Design Elements</i>	40
D.1.	Landscaping: Plant materials and screening.....	40
D.2.	The design of walkways, pathways, and hardscape elements.....	42
D.3.	Residential open space elements.....	44
D.4.	Lighting.....	45
D.5.	Fences, walls, and vertical elements.....	47
E.	<i>Building Design</i>	50
E.1.	General.....	50
E.2.	Architectural character.....	51
E.3.	Architectural composition, massing, and articulation.....	56
E.4.	Pedestrian-oriented facades for mixed-use buildings and transparency for residential buildings.....	63
E.5.	Building details: Entrances, windows and corners.....	64
E.6.	Building facade materials.....	68
E.7.	Blank walls.....	75
E.8.	Requirements for above grade structured parking (multiple user parking garages).....	77

A. Administrative

A.1. Purpose

The general purpose of these Residential Design Guidelines (Guidelines) is to implement the City's Comprehensive Plan vision, especially:

- Goal 12 Policy 8: Conduct a comprehensive review of dimensional and density standards for residential development. Where appropriate increase residential densities to accommodate a wider range of housing types. An increase in housing options and densities should include a review of new design tools and dimensional standards that protect or build upon neighborhood character; and
- Goal 12 Policy 9: Compatible blended density housing: Develop design standards and evaluate appropriate locations for diverse housing types within neighborhoods, utilizing blended densities, and evaluating and recognizing the differences between neighborhoods. The review should look at the character, form, intensity of development, and type of place as well as the mix of uses in the area. Relationships to surrounding neighborhoods are important as tiers or hierarchies of uses are evaluated. This approach to housing is more focused on desired form, with a range of housing types vs. a density-based zoning approach.

More specifically, the purposes of these Guidelines are to:

- Adopt more flexible design standards for residential development that will permit the construction of different housing types compatible with surrounding neighborhoods.
- Promote higher densities along major corridors and in existing neighborhoods already characterized by density.
- Provide clear objectives for those embarking on the planning and design of development projects within Wenatchee.
- Ensure attractive, functional residential development to meet the city's housing needs.
- Promote social and economic vitality.
- Foster safety and comfort through design.
- Promote original and high-quality design.
- Enhance the character and function of Wenatchee's streets.
- Promote building and site design that fits into the context of established neighborhoods.
- Promote sustainable design principles.
- Promote design that enhances the "sense of place" for neighborhoods.
- Increase the awareness of design considerations amongst the Wenatchee community.
- Maintain and enhance property values through appropriate aesthetic and functional design considerations.

A.2. Administrative procedures

The City of Wenatchee Community Development Director (Director) or designee will administer the Wenatchee Residential Design Guidelines (Guidelines), lead the review process, and ensure that new development meets their intent. The review of a development project application with respect to the Guidelines will be the same as, and concurrent with, project review with respect to the zoning provisions. In cases where there is a question of applicability, the Director will make the decision.

A.3. Applicability

The provisions of these guidelines apply to:

A.3.1. Development of new housing types as specified in the table below.

Table A.3.1. Applicability of housing types to these guidelines.

Residential Type	Applicable
Accessory dwelling units, detached accessory dwelling units	No
Single family dwellings	No
Single family cluster	When referenced in Title 10
Single family courtyard	No
Cottages housing	When referenced in Title 10
Duplexes	When referenced in Title 10
Patio homes	When referenced in Title 10
Courtyard housing developments	Yes
Townhouses	Yes
Multifamily dwellings	Yes
Live-work dwellings	Yes
Mixed-use building with at least one residential unit	Yes
Manufactured home communities	When referenced in Title 10
Single purpose commercial buildings	No

A.3.2. Building additions to applicable housing types/elements as specified above.

A.3.3. Exterior modifications (such as facade changes, windows, awnings, signage, etc.) to applicable housing types/elements as specified in the table above.

A.3.4. If there is a discrepancy in these guidelines between the text and figures or charts, the text shall apply.

A.3.5. The conversion of an existing structure to an applicable housing type as specified above.

A.3.6. Storage areas and structures associated with residential developments when referenced in Title 10.

A.4. Terms used in the guidelines

Most sections include the following elements:

- Intent statements, which are overarching objectives, these statements are to be for assistance in determining whether or not the application meets the requirements in the guidelines. Some guidelines offer multiple means of satisfying requirements and allow for proposals that are not specified in the provisions. In these cases, the intent statements provide guidance as to whether these alternative proposals are acceptable.
- Words such as “shall,” “must,” and “is/are required,” signify required actions or features, and must be complied with.
- The use of the word “should” signifies that the provision is required unless there is a compelling reason to the contrary, as determined by the director.
- The use of words such as “is/are recommended,” signify voluntary measures.

A.5. Organization of the residential and mixed-use guidelines

These guidelines are organized into chapters roughly arranged in the sequence of decisions made during the design process. That is, they consider first, the larger site layout parameters that determine the size and configuration of the major project elements such as buildings, parking and circulation. The second section addresses the site design features such as pathways and landscaping that further refine the site layout. The third section covers building design elements that are usually addressed after the larger site elements and building footprints have been determined.

B. Introduction

B.1. Residential development types

As noted in A.1 Purpose, one of the Director's objectives in establishing these guidelines is to encourage the development of a wider spectrum of housing types that in turn offers households more choices in meeting their housing needs, provides housing at different affordability levels, and gives residential developers greater flexibility in taking advantage of different site conditions and market opportunities. At the same time, it is critical that new development fit with its neighborhood context and does not negatively impact its neighbors. To accomplish this, these guidelines and Wenatchee City Code Title 10 (Zoning) include provisions to address development issues specific to each individual housing type.

Chapter 10.08 WCC defines these housing types with respect to specific criteria. These definitions will be used in project review. Chapter 10.10 WCC identifies which housing types are allowed in each zoning district and overlay district. Chapter 10.46 describes the dimensional requirements for development envelopes for each zone, specifying, for example building setbacks, lot coverages and height limits. Chapter 10.47 describes the dimensional requirements for each housing type in each zone and overlay district.

The following sections in B.1 describe the design characteristics, advantages and considerations typical of each type. The contents of section B.1 are for information purposes and are not requirements.

Section B.2 identifies the character areas within the City for the purposes of integrating new development into existing neighborhoods.

B.1.2. Single Family – Courtyard

Description: Two to six single family residences with access from a shared private lane.

Design Characteristics:

- Single family residences with vehicle and pedestrian access from a shared private lane.
- The single family residential lots include yards and entries oriented to the shared private lane rather than the public street.

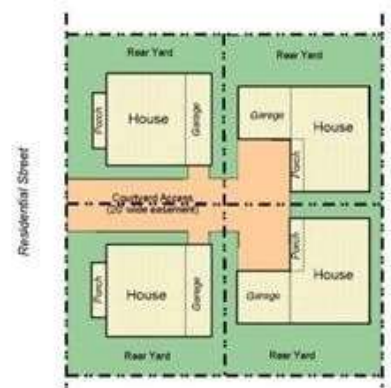
Advantages:

- Allows for more efficient development of larger or unusually configured lots.
- In some cases, it may reduce pavement area and facilitate accommodation of parking spaces.
- Allows fewer curb cuts.
- In some cases, better plat design than skinny lots.
- This configuration could be combined with single family cluster development (i.e. the lot and building could be smaller if allowed by code).

Design Considerations:

- Ensure that the street facing facades of the residences and site improvements provide a pleasant streetscape.
- Ensure accessible and pleasant pedestrian circulation.
- Orient residential entries to be welcoming.
- Minimize the negative impacts of vehicle access and parking.
- Requires access easements and maintenance agreements.

Reference: See also WCC 10.47.070.



B.1.3.B.1.2. Cottage Housing

Description: ~~“Cottage housing” means four or more small, detached individual dwelling units sharing commonly owned open space, courtyard, and parking area. See WCC 10.47.070. Cottage housing development with two to three units is referred to as infill cottage housing. See WCC 10.47.060. Five or more dwelling units with a maximum square footage of 1,200 square feet and a common open space that either (a) is owned in common; or (b) has units owned as condominium units with property owned in common and is a minimum of 200 square feet per unit. Cottages may be in single-family or duplex form. Density is calculated at 0.5 per unit.~~



Design Characteristics:

- Small, usually one to one and a half stories, individual dwelling units with porches and individual gardens near entries.
- Attractive architectural details, materials, and colors that offer some individuality for each unit but lend a continuity for the whole complex.
- Attractive courtyard provides shared open space.
- Parking can be individual spaces, ~~or in~~ a combined lot around perimeter, or garages.



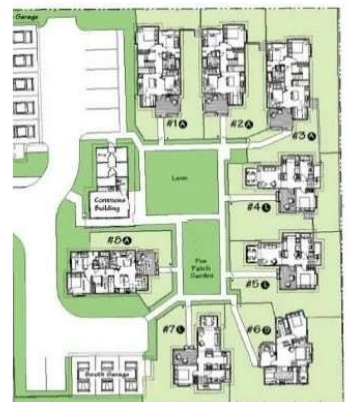
Advantages:

- Increased density without the bulk of large buildings.
- Offers small unit living within an individual structure.
- Orientation around a courtyard offers social setting and security.
- Generally more affordable than a traditional single family residence.
- Usually compatible with single family neighborhoods.



Design Considerations:

- Privacy and security should be addressed in site design.
- Relationship between the cottages and open space must be welcoming but provide privacy.
- Architectural detailing and landscaping is important to ensure quality.
- Parking location and site design should minimize impacts.



Shared parking and individual lots of this example.

Reference: ~~For four or more units, see See WCC 10.47.090. For infill cottage housing options with three or fewer lots, see WCC 10.47.080.~~

B.1.4.B.1.3. Duplexes and Patio Homes

Description: ~~A detached residential building designed for occupancy by two self-contained attached dwelling units living independently of each other (WCC 10.08.055 “D”).~~

~~Patio homes refer to a duplex where the dwelling units are placed side by side and each unit is platted on its own lot (WCC 10.08.115). two attached dwelling units, where there are five total units on one lot.~~

Design Characteristics:

- Duplexes may look like a single-family residence, especially if there is a single driveway or the drives access from an alley.
- Corner lots offer excellent opportunities for ground related duplexes.
- Dwelling units may be situated side by side or one over another.
- Stacked duplexes (one unit over another) can tend to be – but are not necessarily – bulkier than single family residences.
- Some existing two-story houses can be successfully converted to a stacked duplex.
- New duplexes can feature a variety of styles and configurations.

Advantages:

- Added density without changing the character of a single-family neighborhood.
- Tends to be more affordable per unit than single family residence.
- Offers home buyer the ability to reduce monthly cost with revenue from rental unit.

Design Considerations:

- Duplexes are the most compatible in single family neighborhoods when they have the same architectural character as their neighbors.
- Building elements and details are important to prevent duplexes from looking “boxy”.
- It is especially important to minimize driveways and curb cuts.
- Front yards, entries, and drives must be configured to create a pleasant streetscape.
- Exposed parking under the building and visible from the street should be avoided.

Reference: See WCC 10.47.100–110 for duplex and patio home standards.



Duplexes work well on corner lots.



Older houses can be successfully converted to duplexes if parking can be accommodated.



Contemporary styled duplexes are possible.

B.1.5.B.1.4. Townhouses

Description: ~~A single family dwelling with an individual entry onto a street or common walkway that shares at least one wall and a property line with another dwelling. Townhouses are typically individually owned ground related units. See WCC 10.47.130. A row of at least three attached dwelling units, where there are at least five total units on one lot, and constructed to a townhouse standard per International Residential Code (IRC) R302 as amended. Each unit has its own front access to the outside, no unit is located over another unit, and each unit is separated from any other unit by one or more common walls.~~

Design Characteristics:

- Townhouses generally feature a compact footprint and multiple stories.
- Townhouses may be aligned in a straight row facing a street or configured around a courtyard or common pathway.
- The front facade of townhouses may feature building modulation to break up the building's massing.
- Front yards of townhouses are often relatively small.

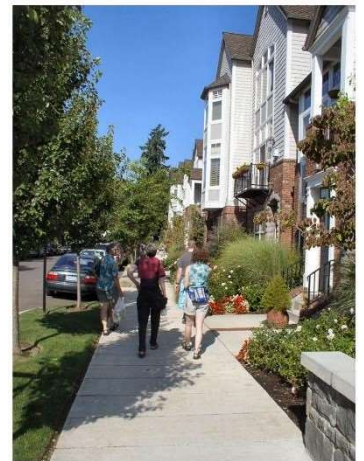
Advantages:

- Added density and affordability.
- Usually individually owned.
- Can provide an individual backyard.
- Can provide a pleasant streetscape and transition between public right of way and interior privacy.

Design Considerations:

- Because of townhouses' relatively narrow width, it is important that driveways and automobile access from the main street are minimized or should drive so they do not dominate the front facade (alley access is preferred).
- If the front townhouse facades are situated near a public street, the ground floor should be elevated and/or set back from the sidewalk to maintain the residents' privacy.
- Building elements, details, and modulation can help reduce the building's massing.
- The color or detailing may vary from one unit to the next to add individual identity to the residences.

Reference: See ~~also~~ WCC 10.47.130.



B.1.6.B.1.5. Courtyard Housing

Description: ~~Multifamily dwelling units surrounding a courtyard, pathway with landscaping, or other open space. Three or more multifamily dwelling units, where there are at least five total units on one lot, arranged on two or three sides of a yard or court which is preserved as common open space. Density may be reduced to 0.75 per unit for units 1,000 square feet or less.~~

Design Characteristics:

- Generally, the units are ground related.
- The central open space may be lawn, a passive open space with a garden or landscaping, or an active open space with, for example, playground equipment.

Advantages:

- Allows higher densities and more affordable units.
- Offers a unique residential type.
- The open space can be very attractive and useful.

Design Considerations:

- The design of the open space and the transition between public open space and the private spaces around the units are very important.
- Impacts of parking should be minimized.
- The complex should present an attractive street front.

Reference: See ~~also~~ WCC 10.47. [120070](#).



B.1.7.B.1.6. Multifamily

Description: ~~Buildings with three or more attached units. Such buildings may have common or individual entrances and could be rental apartments or condominiums. Three or more attached dwelling units, where there are at least five total units on one lot.~~

Design Characteristics:

- Generally aligned parallel to the street front but may be arranged around an internal courtyard. See courtyards or condos above.
- Upper story facades include elements such as balconies, modulation, and clusters of windows to break up the building’s massing.

Advantages:

- Offers higher density and amenities such as a roof deck, views, etc.
- Generally, more affordable than other housing types.
- Appropriate in places where a less established residential neighborhood context is offset by convenience and internal amenities.

Design Considerations:

- Measures should be taken to ensure livability of dwelling units at or near grade.
- The front facade of the building should incorporate building elements, articulation, attractive materials, and details.
- The front yard landscaping should provide privacy to ground floor units and screen parking, as well as add to a usable streetscape.
- Some residential open space should be provided. This may take many forms (outdoor courtyard or other open space, roof deck, balconies, exercise room, etc.).
- The entry(s) should be welcoming and secure.
- The privacy and solar access of neighboring residences in adjacent lower intensity zones should be protected by building setbacks and upper story step-backs, if necessary.

Reference: See ~~also~~ WCC 10.47.150.



B.1.8.B.1.7. Mixed-Use

Description: ~~For the purpose of these guidelines, a “mixed use building” refers to a building that features a non-residential use or uses on at least a portion of the ground floor and at least one dwelling unit. Buildings that combine residential uses with permitted nonresidential uses.~~

Design Characteristics:

- Ground floor storefront use feature weather protection, transparency (ample window area), commercial ceiling height, attractive details, and a welcoming entry to encourage pedestrian activity.
- Upper stories feature elements, characteristics, and materials that provide a “human scale” and attractive facade.

Advantages:

- Offers very convenient services and pleasant street level activity.
- Usually relatively affordable.
- Higher density of all residential types and reduces pressure on single family neighborhoods.
- May offers views.
- Concentrations of residents support local businesses.

Design Considerations:

- The front facade of the building should incorporate building elements, articulation, attractive materials, and details.
- Some residential open space should be provided. This may take many forms (outdoor courtyard or other open space, roof deck, balconies, exercise room, etc.).
- The entry(s) should be welcoming and secure.
- The privacy and solar access of neighboring residences in adjacent lower intensity zones should be protected by building setbacks and upper story step-backs, if necessary.

Reference: See ~~also~~ WCC 10.47.160.



B.1.9.B.1.8. Live-Work Dwellings

Description: ~~WCC 10.08.095 “L” defines “live-work dwelling” as a~~ dwelling units designed to accommodate a small commercial enterprise on the ground floor and a residential unit above and/or behind.

Design Characteristics:

- Generally, two- to three-story buildings with a large multipurpose space near the front entrance that can accommodate commercial, retail, or office activities.
- The commercial space shall comply with accessibility requirements.
- Two or more units may occupy a single building in a townhouse type of configuration, or they may be arranged around a shared access point.
- Sometimes the units are flexible so that the ground floor can function as a ground related unit or as a work space, depending on the occupant’s needs.

Advantages:

- Offers a unique opportunity for small businesses.
- Its small scale can fit within some neighborhoods or provide a transition between a commercial district and a single family neighborhood.
- Offers flexibility for the owner.

Design Considerations:

- Parking should be carefully considered as the unit may need space for both residents and clients.
- Care should be taken so that the unit doesn’t become just a townhouse without privacy for the resident.
- Signage and commercial access should be addressed.

Reference: See ~~also~~ WCC 10.47.140.



B.2. Establishment of “Character Areas”

One of the objectives of these guidelines is to support and enhance the unique design characteristics of specific neighborhoods or areas within the city. One way to do this in the design of new residential development is to emphasize the architectural design characteristics of the locale in which it is built. To implement this goal the following “character areas” are established as described below and on the map in Figure B.2.a.

Core Residential Neighborhoods. All properties in the RM and RH zones east of Western Avenue, north of Crawford Avenue, and south of the Wenatchee River. The area also includes properties in the OMU zone.

Perimeter Residential Neighborhoods. All properties in the RS, RL and RF zones south of the Wenatchee River plus all RM, RH, and SWBD zoned properties south of Crawford Avenue. The area also includes the OMU zoned areas west of Western Avenue.

Sunnyslope. All residentially zoned properties north of the Wenatchee River.

North and South Wenatchee Business Districts. All properties in the NWBD and SWBD zones north of Crawford Avenue plus adjacent areas in the residential mixed-use zoning district.

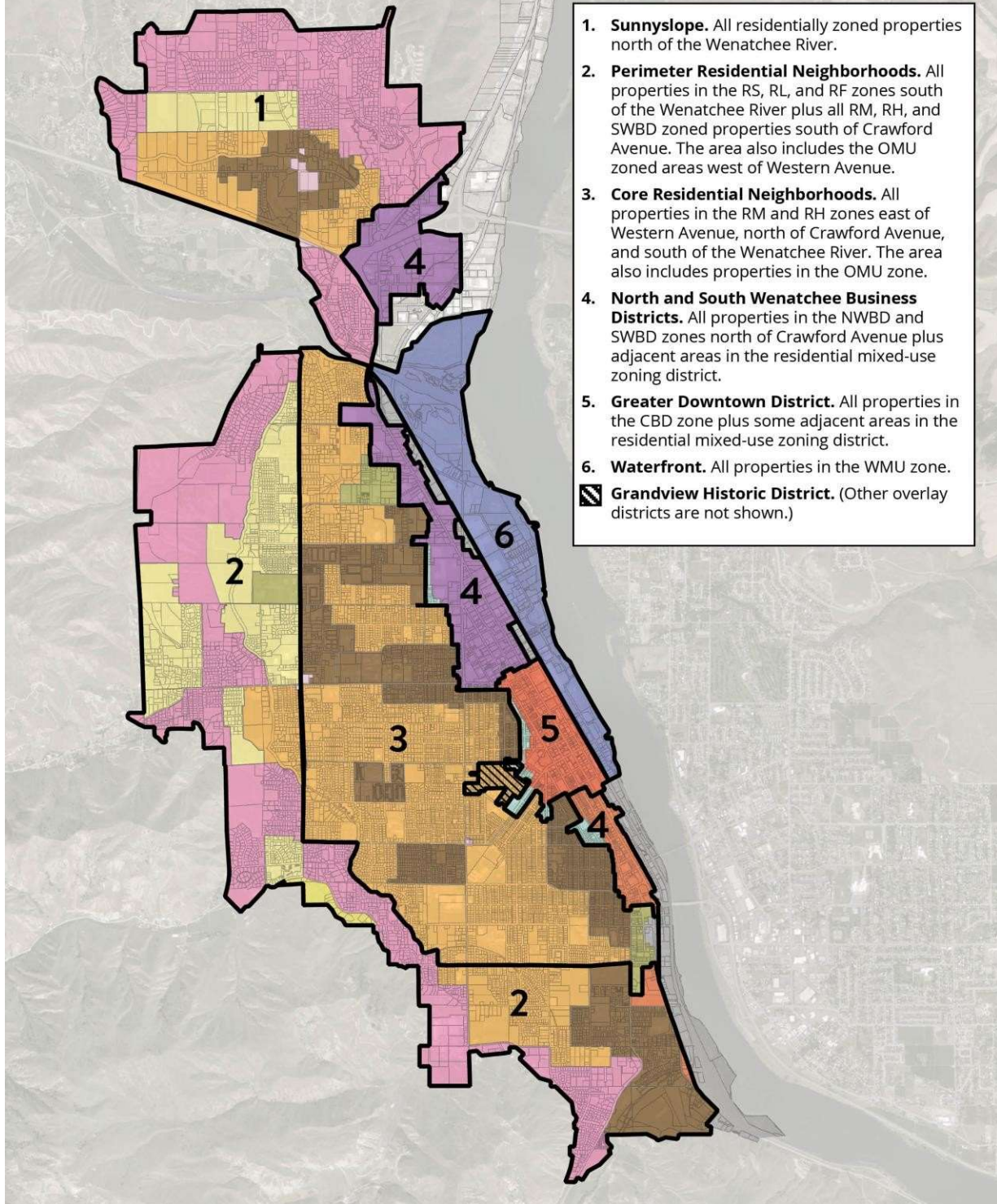
Greater Downtown District. All properties in the CBD zone plus some adjacent areas in the residential mixed-use zoning district.

Section E.2.1 Architectural design character and E.6.2 Building materials and colors include guidelines that apply to specific character areas.

B.3. Grandview Historic District

The guidelines related to design character in Sections E.2.1 and E.6.2 do not apply to properties in the Grandview Historic District. Projects in the historic district shall be reviewed in accordance with WCC 10.40.060.

WENATCHEE RESIDENTIAL CHARACTER AREAS



1. **Sunnyslope.** All residentially zoned properties north of the Wenatchee River.
 2. **Perimeter Residential Neighborhoods.** All properties in the RS, RL, and RF zones south of the Wenatchee River plus all RM, RH, and SWBD zoned properties south of Crawford Avenue. The area also includes the OMU zoned areas west of Western Avenue.
 3. **Core Residential Neighborhoods.** All properties in the RM and RH zones east of Western Avenue, north of Crawford Avenue, and south of the Wenatchee River. The area also includes properties in the OMU zone.
 4. **North and South Wenatchee Business Districts.** All properties in the NWBD and SWBD zones north of Crawford Avenue plus adjacent areas in the residential mixed-use zoning district.
 5. **Greater Downtown District.** All properties in the CBD zone plus some adjacent areas in the residential mixed-use zoning district.
 6. **Waterfront.** All properties in the WMU zone.
- ▨ **Grandview Historic District.** (Other overlay districts are not shown.)

Figure B.2.a. Character area boundaries.

C. Site Planning

C.1. Relationship to street fronts and common pathways

INTENT

- To provide for the privacy, comfort, and livability of the residential units.
- To provide an attractive streetscape.
- To allow for friendly communication between residents in an outdoor space and pedestrians on the sidewalk.
- To provide an inviting entry into the units.
- To foster pedestrian-oriented businesses and activities in mixed-use buildings on pedestrian-oriented streets.

GUIDELINES

C.1.1. Ground-related units facing streets

- Applicability.** This guideline applies to all ground-related residential units, as defined in 10.08 WCC, facing a public street and located within the following zoning districts and overlays:
 - North Wenatchee Business District (NWBD)
 - South Wenatchee Business District (SWBD)
 - Central Business District (CBD)
 - Waterfront Mixed Use (WMU)
 - Neighborhood Commercial (CN)
 - Historic Entertainment overlay (HEO)
 - Waterfront Pedestrian overlay (PO)
 - Waterfront Recreational/Residential overlay (RRO)
- Purpose.** Provide for internal privacy for people living in the ground-related units.



Figure C.1.a. Desirable ground-related residential example with raised unit, landscaping, and small porches to enhance privacy while providing a welcoming streetscape. Similarly, these measures add privacy and a sense of community.

- c. **Street access.** Ground-related residences fronting a street shall either have individual ground-related entries or a shared entrance oriented and accessible to the street.
- d. **Set back or elevate units for privacy.** Setbacks of less than 15 feet (where allowed by WCC Chapter 10.46) warrant pro-active design treatments to create an attractive and effective transition between the public and private realms.
 - i. If the front facade is 10-15 feet from the public street right-of-way, elevate the ground floor unit at least 30 inches above grade and elevate the bottom of ground floor windows facing the street at least 5 feet above grade.

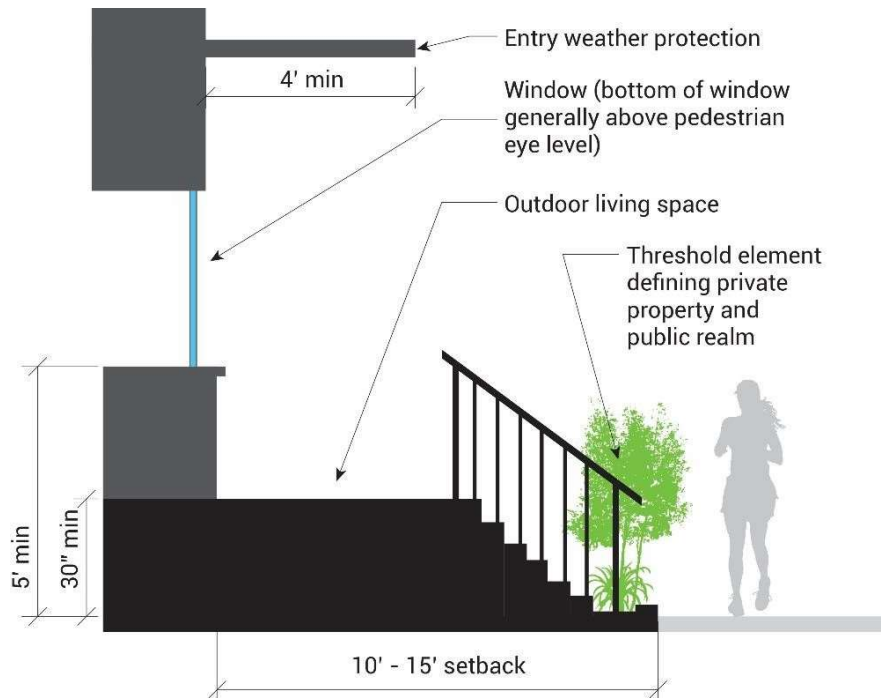


Figure C.1.d.i. The above provisions apply to ground-level residential frontages with 10 to 15-foot setbacks. The below provisions apply to ground-level residential frontages with setbacks less than 10 feet.

- ii. If the front facade is 5-10 feet from the public street right-of-way, elevate the ground floor unit at least 3 feet above grade and elevate the bottom of ground floor windows facing the street at least 6 feet above grade.

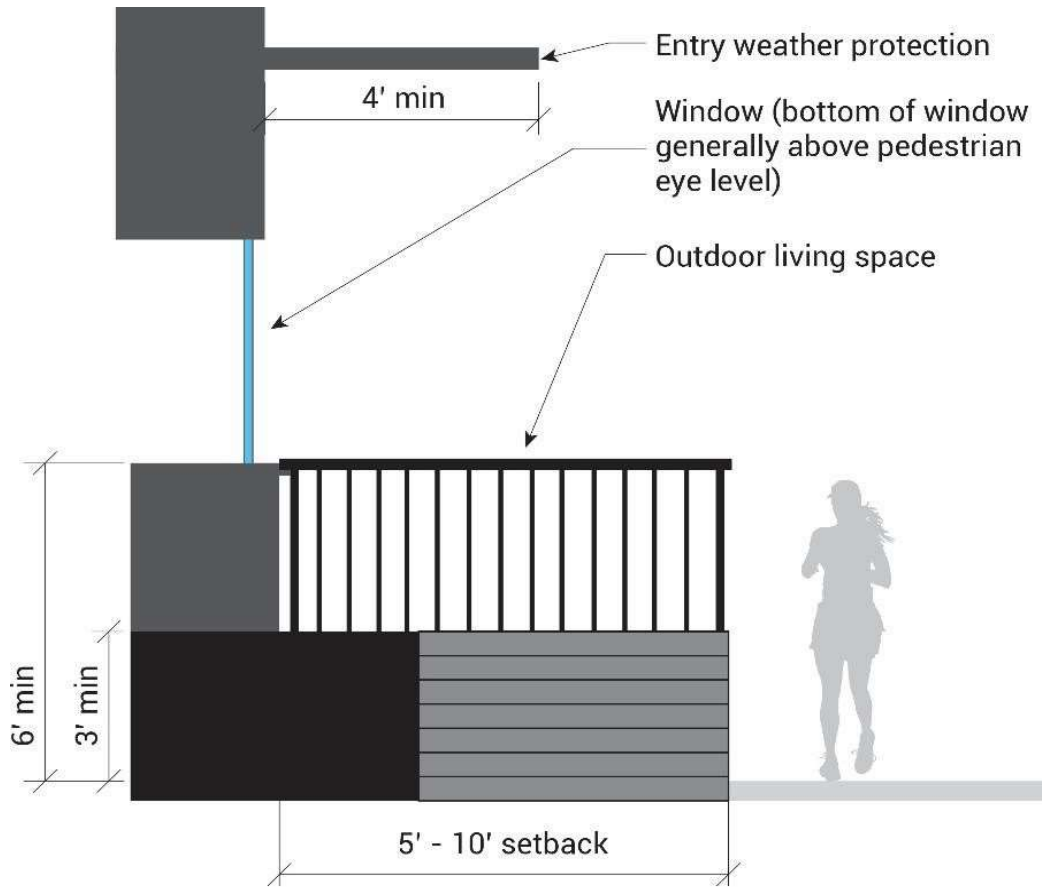


Figure C.1.d.ii. Dimensional relationships to maintain privacy and provide a successful transition between public and private realms when setbacks are reduced below 10 and 15 feet.



Figure C.1.c. The above images show ground-level residential frontages with setbacks of approximately 10 feet (left image) and 5 feet (right image) along different street frontages for this corner apartment building. These ground level units all have their own private unit access from the sidewalk and are elevated above the sidewalk to enhance the privacy to the units. The landscaping elements, brick posts, split-faced concrete block stoop walls, and black metal railings help to provide an attractive and effective transition between the public and private realm within the adjacent dwelling units.

- e. Ground related units setback less than 15 feet** shall include the following design features:
- i. Provide a physical “threshold” feature such as a hedge, retaining wall, rockery, stair, gate, railing, or a combination of such elements on private property that defines and bridges the boundary between public right of way and private entry, porch, yard, or patio. Thresholds may screen but must not block views to and from the street or common pathway. Retaining walls adjacent to a public ROW shall be no taller than 30 inches. If additional height is required to accommodate grade conditions, then accommodate the grade change according to D.5.1.
 - ii. Provide an outdoor space at least 4 feet in depth and 6 feet wide (24 square feet minimum) in the front setback such as a porch, patio, deck, or stoop. Where feasible, this space must be at the same level as the interior of the unit. The Director may allow an exception for an outdoor space with other dimensions if the space meets the intent of providing a transition between the street and the private space.

- iii. Provide a covered area, porch or protected entry space, or other architectural weather protection at least 4 feet deep and 4 feet wide (parallel to the street or pathway), that provides cover for a person entering the unit and a transitional space between outside and inside the dwelling.
- iv. Landscaping planters shall be integrated into transitional areas between the dwelling unit and the adjacent sidewalk, pathway or open space to enhance the transition between public and private realm (see Figure C.1.c below for an example). Where the ground level dwelling unit setback is more than ten feet, a landscape planter shall be integrated between the sidewalk, pathway, or open space and the porch, patio, deck, or stoop. Where the ground level dwelling unit setback is less than ten feet, a landscape planter shall be integrated between dwelling units/stoops.



Figure C.1.d. Ground floor residential units such as these often lack privacy and the livability of outdoor spaces is limited. Locating the ground floor at or close to grade or with insufficient setback results in an uninviting space even with the planter in place. This condition should be avoided.

C.1.2. Street fronts on pedestrian-oriented streets

- a. **Applicability.** This guideline applies to all development facing a designated pedestrian-oriented street. (See Figure C.1.e)
- b. **Pedestrian-oriented facade.** Feature “pedestrian-oriented facades” as described in Section E.4.1 Pedestrian-Oriented Facades.
- c. **Ground floor use.** Feature non-residential ground floor uses. Hotel and multifamily residential lobbies, common rooms and entries are allowed.
- d. **Ground floor setbacks.** Feature ground floor setbacks at least 12 feet from the face of curb. Upper stories may extend to the property/right of way line.
- e. **Street wall definition.** The ground floor shall extend to the property/right of way unless the setback from the curb required in “d” is applicable. (For example, align the building along the right of way unless the resulting sidewalk area is less than 12 feet wide from back of curb to the first floor building wall at grade.)

Exception: A building front (measured parallel to the right of way line) may be set back up to 60 feet from the right of way provided the setback is occupied by a plaza, landscaped area or other space that invites pedestrian activity and buildings facing the open space meet the Pedestrian Facades and Weather Protection requirements in Section E.4.1.

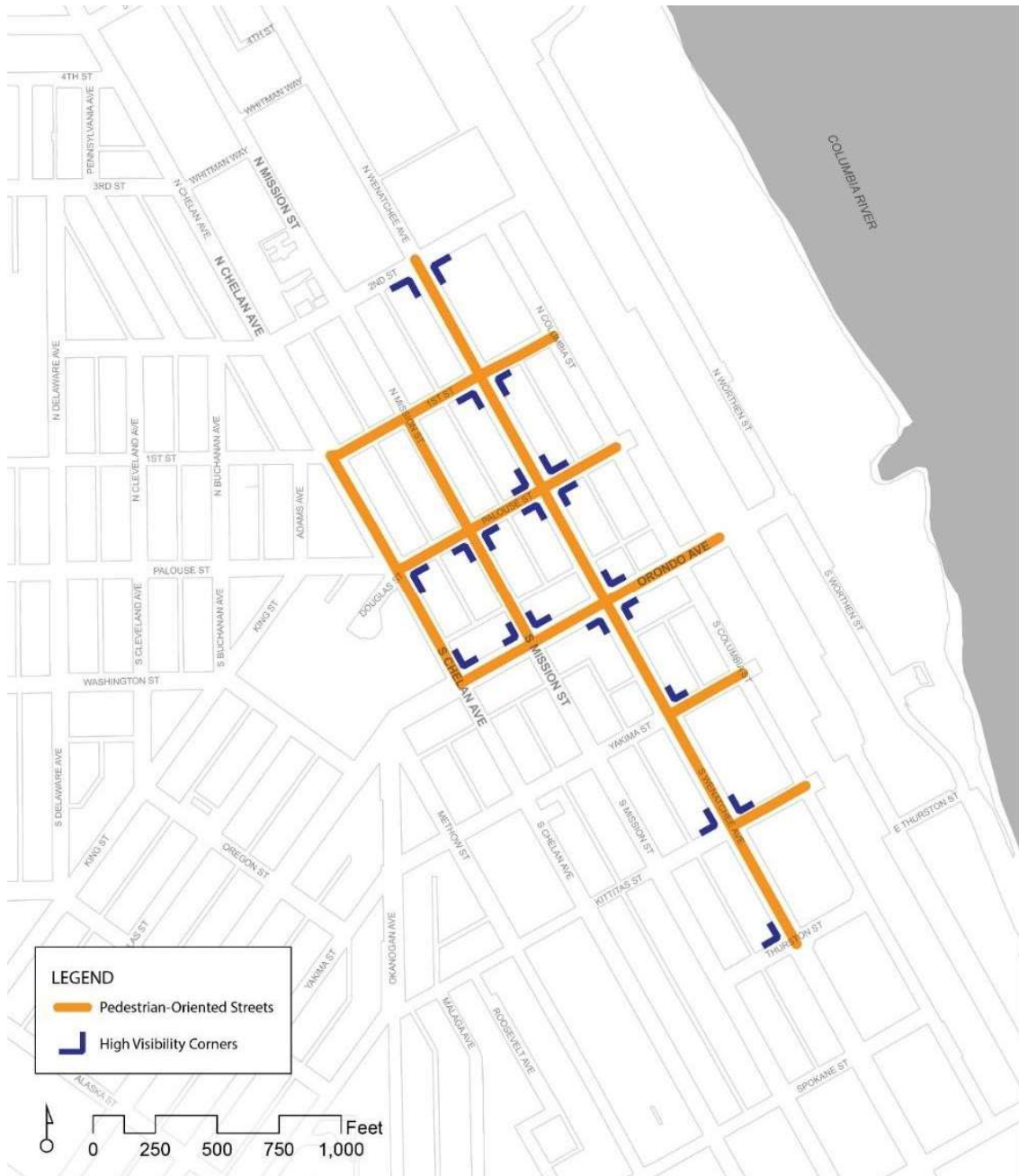


Figure C.1.e. Designated pedestrian-oriented streets.

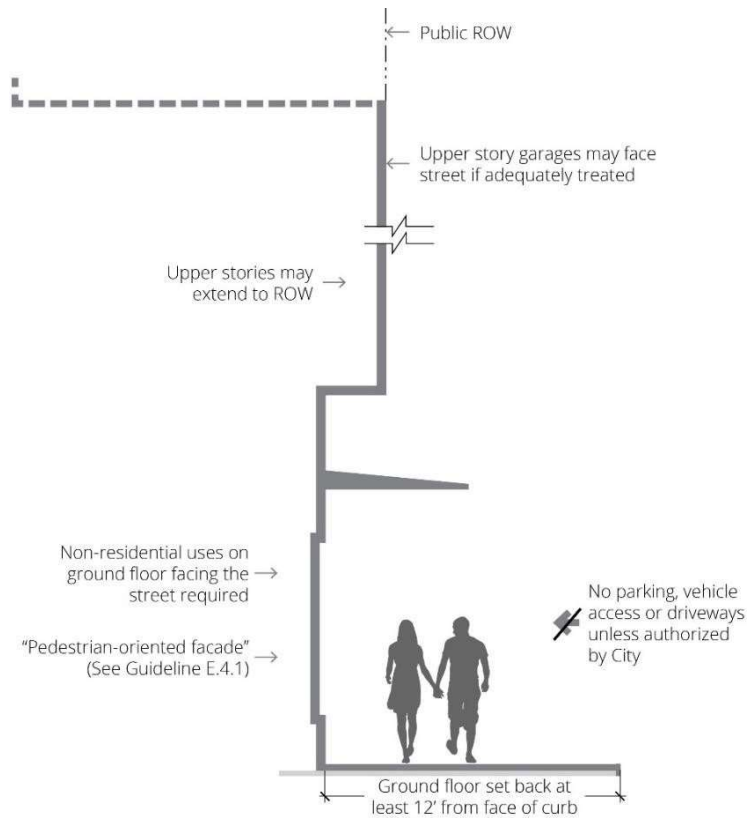


Figure C.1.f. Summary of requirements for building facades facing a designated pedestrian-oriented street. (This diagram is for illustrative purposes only. See Section C.1.2 for stated requirements.)

- ~~f. **Vehicle parking.** Surface vehicle parking directly fronting, and ground floor structured parking directly adjacent to a designated pedestrian-oriented street is prohibited. The Director may allow upper story parking facing the street if the facade is suitably treated to appear as an occupied building or incorporates articulation treatments that break up the massing of the garage and add visual interest as described in Section E.7.1. The Director may also allow structured parking to face a designated pedestrian-oriented street if the subject property faces two or more designated pedestrian-oriented streets and the parking is treated per Section E.8.~~
- ~~g. **Vehicle access (driveways).** All vehicle access shall be from another street or alley unless the Director determines that there is no other way to provide safe vehicle access. For example, if the property is on the corner of two designated pedestrian-oriented streets or fronts a designated arterial and a designated pedestrian-oriented street, the Director will determine which street fronts may feature vehicle access (a driveway).~~

C.1.3. Location and configuration of parking facilities facing a street front

- a. — Applicability.** This guideline applies to all parking lots serving more than two residential units. This guideline does not apply to parking lots that only face an alley or private roadway. If an exception is granted in WCC10.60.030 it is also granted here.
- b. — Parking lot location.** All parking must be located beside, behind, underneath, or above the ground floor use facing the street (i.e., no parking is allowed between the building and the street). Parking is limited to 50 percent of the street front or 65 feet, whichever is narrower. Parking areas serving more than two residential units along the street must be screened (See Section D.1 and Chapter 10.62 WCC).

C.2. Pedestrian and non-motorized circulation

Note: This section addresses requirements for the layout and configuration of pedestrian circulation within the site such as walkways, trails, internal sidewalks. See Section D.2 for design elements associated with pedestrian movement such as lighting, paving, adjacent landscaping, etc.

INTENT

- To improve the pedestrian environment by making it easier, safer, and more comfortable.
- To provide pedestrian access to transportation resources such as sidewalks, bikeways, crosswalks, and bus shelters connecting to all modes of transportation.
- To provide convenient pedestrian circulation connecting all on-site activities to adjacent pedestrian routes and streets.

GUIDELINES

C.2.1. Pedestrian connectivity within multi-unit residential complexes

- a. Applicability.** This guideline applies to applicable residential development with more than one building.
- b. Pedestrian paths.** Provide safe, accessible pedestrian paths or sidewalks from the primary residence entry to a primary internal street, public street, or common open space. Pathways used to satisfy this requirement must conform to the provisions in Section D.2.
- c. Pedestrian network.** The pedestrian circulation system shall connect all main entrances on the site. For duplexes, triplexes, fourplexes, and townhouses or other residential units fronting the street, the sidewalk may be used to meet this standard. For multifamily developments, pedestrian connections to other areas of the site, such as parking areas, recreational areas, common outdoor areas, and any pedestrian amenities are required.
- d. Bicycle facilities.** In multi-story multifamily buildings where some units are not ground related, provide at least one secure bicycle storage space for each unit. The storage space may be within a structured parking area, special structure or within the dwelling unit, if approved by the Director.

C.3. Residential open space

INTENT

- To create useable space that is suitable for leisure or recreational activities for residents.
- To create open space that contributes to the residential setting.

GUIDELINES

C.3.1. Amount of required “residential open space”

See WCC Chapter 10.47 Residential Use Standards for open space required for different residential development types.

C.3.2. Requirements for different types of required residential open space

- a. **Applicability.** This guideline applies to open space for housing types applicable to these guidelines and as set forth in WCC Chapter 10.47. Residential developments may include other types of open space that do not qualify as required open space such as wider landscaped buffers or critical areas. However, these features do not count as “residential open space”.
- b. **Requirements for common open space.** To qualify as common open space for the purpose of meeting open space standards for applicable housing types described in WCC Chapter 10.47 such open space shall:
 - i. Have no dimension less than 15 feet.
 - ii. Be of a grade and surface suitable for complying with provisions below.
 - iii. Be on the site of the proposed development.
 - iv. Conform to minimum dimensional requirements described in WCC Chapter 10.47 for the various housing types.
 - v. Not be located within: the required street setback, any required landscaped areas, nor areas devoted to parking or vehicular/pedestrian access (including sidewalks required for meeting the pedestrian circulation requirements). Exception: Common open space may include areas within a setback if that open space meets all the other requirements of Section C.3.2.
 - vi. Be centrally located and designed to be conveniently accessible to all residents from the interior of the development.
 - vii. Include at least 75 percent “usable open space”. Usable open space includes open play areas and outdoor recreational features, trails and paths, community gardens, walkable lawns and other similar types of areas. Private balconies, patios, or decks as well as inaccessible open spaces specifically do not qualify as common open space.
 - viii. Common open space shall be separated from ground level windows, streets, service areas and parking lots with landscaping, low-level fencing, or other treatments as approved by the Director that enhance safety and privacy for both the common open space and dwelling units. See also requirements for ground related units in Section C.1.1.

- ix. When possible, the space shall be oriented to receive sunlight, face east, west or preferably south, when possible.



Figure C.3.2.a. Four examples of common space illustrating that such space can serve a variety of uses.

- c. **Requirements for balconies counted as useable open space.** To be counted as useable open space, balconies must be at least 6 feet wide by 6 feet deep.
- d. **Requirements for roof decks counted as useable open space.** To be counted as useable open space rooftop decks must:
 - i. Be at least 15 feet by 15 feet - and at least 225 square feet in area.
 - ii. Include amenities and spaces for activities such as dining, resting, parties, etc.
 - iii. Include protection to mitigate weather conditions such as wind, sunlight, and rain.
 - iv. Include landscaping (e.g.: container plantings), artwork or other enhancements.
 - v. Be accessible to all units.



Figure C.3.2.b. Two examples of acceptable rooftop decks.

- e. **Requirements for interior spaces counted as usable open space.** To be counted as usable open space the room or interior space must be open and accessible to all units and include furniture or equipment to support recreational activities such as physical exercise, dance, or group functions, games, meetings, dining, etc.



Figure C.3.2.c. Examples of acceptable interior open spaces.

C.4. Vehicle access and parking

INTENT

- To allow for residential infill that is compatible with the character and scale of existing residential neighborhoods without adversely impacting neighbors.
- To enhance the character of the street and neighborhood.
- To maintain “eyes on the street” for safety to pedestrians and to create a more welcoming and interesting streetscape.
- To de-emphasize garages and driveways as major visual elements along the street.
- To provide safe and efficient vehicular access to residences.
- To minimize conflicts with pedestrian circulation and activity.

GUIDELINES

C.4.1. Vehicular access – general

- Applicability.** This guideline applies to open space for housing types applicable to these guidelines and as set forth in WCC Chapter 10.47. For development fronting on designated pedestrian-oriented streets, see Sections C.1.2. e & f above.
- Street access.** All vehicle access such as driveways shall be from an alley if one is available and the Director determines that the alley is sufficiently sized, configured and paved for vehicular access. If no alley is available, then access shall be from the street with the lower street classification or less traffic, as determined by the Director.
- Parking facilities requirements.** Parking facilities shall be provided in accordance with WCC Chapter 10.60.
- Garages.** Garages facing a public street (not an alley) must be set back in accordance with WCC Chapter 10.46. See also Section E.7 for garage and parking facility design requirements.
- Parking and Landscaping.** The location and configuration of parking facilities and associated landscaping shall be as approved by the Director and meet the requirements of WCC 10.48.130 (clear view triangle).

C.4.2. Vehicular access for residences with individual driveways and parking facilities

See WCC Chapters 10.46 and 10.47 for requirements specific to different zones and residential types.

C.4.3. Vehicular access for residences with driveways and parking facilities serving multiple residences (e.g.: parking lots for multifamily structures, courtyard complexes, etc.)

- a. **Applicability.** This guideline applies to all developments where automobile access and parking is shared by multiple residential units.
- b. **Driveway width.** Driveways serving multiple residences shall meet WCC 3.16. Fire Code, including Appendix D, Fire Apparatus and Access Roads, and shall not be less than 20 feet wide and not more than 24 feet wide.
- c. **Driveway length.** Driveways shall be sufficiently long to accommodate a vehicle parked between the right of way and the garage without blocking the adjacent sidewalk. (Typically 18 feet minimum.)
- d. See also Section C.1.3. Location and configuration of parking facilities facing a street front.

C.4.4. Streetscape design of internal roadways

- a. **Applicability.** This guideline applies to all developments where automobile access requires internal roadways other than from a public right of way to a single parking area or structure.
- b. **Internal streetscape design.** To increase the function and appearance of internal roadways on sites greater than two acres, street trees and sidewalks at least 5 feet wide must be provided on all internal access roadways, excepting access roads designed solely for the purpose of service (e.g. waste pick-up) and loading.

C.5. Site planning for security

INTENT

- To increase personal safety and property security.
- To discourage property damage and vandalism.

GUIDELINES

C.5.1. Provisions to increase safety and security – features to avoid

- Applicability.** This guideline applies to housing types applicable to these guidelines and as set forth in WCC Chapter 10.47.
- Avoid entrapment areas (where a person could become trapped with no exit route).** Provide two means of egress from all outdoor spaces. Also ensure that entrapment conditions are avoided in the design of rooftop decks and service areas.

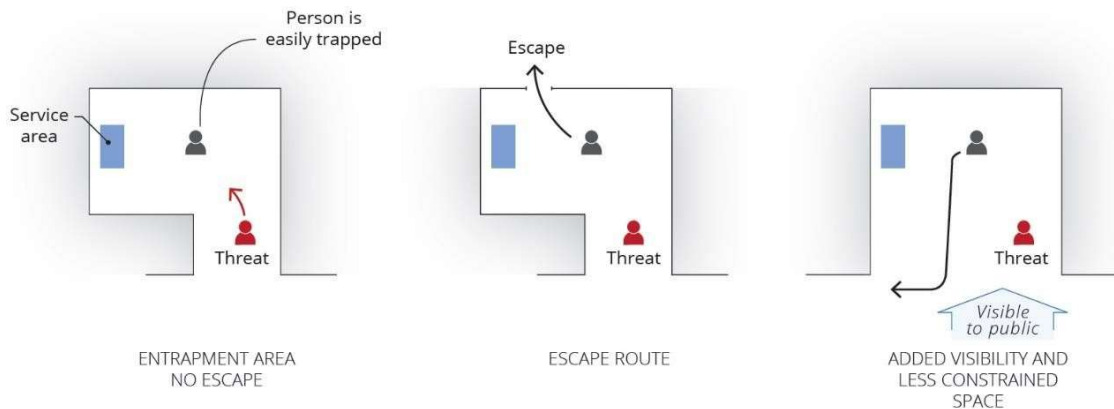


Figure C.5.1.a. Measures to avoid entrapment.

- Visibility.** Avoid areas that are dark or not visible from a public space or right-of-way.
- Sight obscuring elements.** Avoid vegetation and fences that restrict visibility into occupiable open space, pathways and building entries and buildings, vegetation, or other objects (e.g., a storage enclosure) that block visibility into a space or provide places to hide. Where visibility is necessary to avoid creating an unsecure area to reduce the potential for pedestrian/vehicle collisions, do not plant vegetation that will obstruct views between three feet and eight feet above the ground.

C.5.2. Provisions to Increase Safety and Security – Features to Include

- a. **Applicability.** This guideline applies to housing types applicable to these guidelines and as set forth in WCC Chapter 10.47.
- b. **“Passive surveillance,”** the ability of people occupying buildings and public spaces to view all parts of accessible spaces.

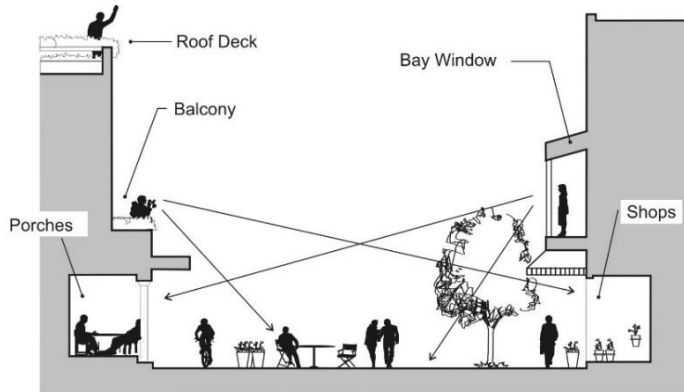


Figure C.5.1.b. Passive surveillance conditions.

- c. **Lighting.** Provide security and pedestrian lighting per Section D.4.
- d. **Appropriate natural access control.** Provide features that delineate where the general public should not enter without an invitation. For example, a low fence or hedge (two-four feet high) can indicate that people should not enter a yard or open space except through an opening. Access control shall not limit visibility or passive surveillance.
- e. **Spatial definition of residential territory.** This means clearly indicating through site planning and design measures what parts of the site are open to the public and what parts are not. For example, in residential development, pedestrian-oriented elements and walkways indicate that the public is welcome but fenced areas with a gate do not. Also, well maintained sites indicate that someone cares for the site and tends to discourage crime.



Figure C.5.1.c. Two examples of access control and spatial definition. The gateway and landscaping of the left example clearly indicates that the courtyard is a private space while still being welcoming. The pathway and courtyard complex on the right provides levels of privacy through the gateway structure and more actively with the gate in the rear ground.

C.6. Relationship to adjacent properties

INTENT

- To protect the privacy of residents on adjacent properties.
- To promote the functional and visual compatibility between developments.

GUIDELINES

See also: WCC 10.46.080.

C.6.1. Balconies near side and rear property lines adjacent to property in any residential zone

- Applicability.** This guideline applies to housing types applicable to these guidelines and as set forth in WCC Chapter 10.47.
- Balconies and rooftop decks overlooking residentially zoned property.** Balconies and rooftop decks above the ground floor within 15 horizontal feet of a side property line abutting a residentially zoned property must feature a railing system that is at least 50 percent opaque. Specifically, 50 percent of the area below the top edge of the railing must be a sight-obscuring structure.

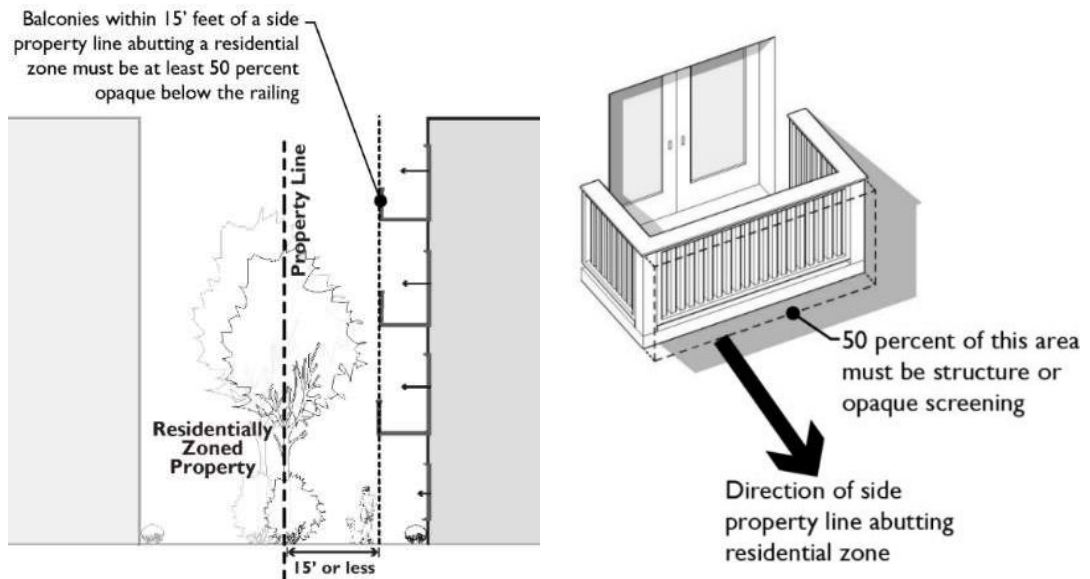


Figure C.6.1.a. Privacy standards for balconies within 15 feet of side or rear property lines.

C.7. Service areas and mechanical equipment

INTENT

- To minimize adverse visual, odor, and noise impacts of mechanical equipment, utility cabinets and service areas at ground and roof levels.
- To provide adequate, durable, well-maintained, and accessible service and equipment areas.
- To protect residential uses and adjacent properties from impacts due to location and utilization of service areas.

GUIDELINES

C.7.1. Location of ground related service areas and mechanical equipment

- Applicability.** This guideline applies to housing types applicable to these guidelines and as set forth in WCC Chapter 10.47 when the site includes collective or joint use ground related service areas or mechanical equipment such as loading docks, trash dumpsters, storage areas, compactors, recycling areas, electrical panels, and mechanical equipment.
- Service areas.** Service areas must be located for convenient service access while avoiding negative visual, auditory, olfactory, or physical impacts on the streetscape environment and adjacent residentially zoned properties. Service areas must be sited for alley access if available.

The Director may require evidence that such elements will not significantly impact neighboring properties or public areas. (For example, the Director may require noise damping specifications for fans near residential zones.)

- Visibility of service areas.** Service areas must not be visible from the sidewalk and adjacent properties. Where the Director finds that the only option for locating a service area is an area visible from a street, internal pathway or pedestrian area, or from an adjacent property, the area must be screened with structural and or landscaping screening measures provided in Section C.7.2 below and Chapter 10.62 WCC Landscaping.
- Design for safety.** Other provisions of this guideline notwithstanding, service areas used by residents must be located to avoid entrapment areas and other conditions where personal security is potentially a problem. See Guidelines in Section C.5. The Director may require pedestrian-scaled lighting or other measures to enhance security.
- Noise from mechanical equipment.** Locate and/or shield noise producing mechanical equipment such as fans, heat pumps, etc., to minimize sounds and reduce impacts at property lines adjacent to residentially zoned properties. In no case shall noise exceed the standards in WAC173-60.

C.7.2. Screening of ground related service areas and mechanical equipment

- a. **Applicability.** This guideline applies to applicable developments that include collective or joint use service areas or mechanical equipment and where the adverse effects of those elements cannot be adequately mitigated through location.
- b. **Structural enclosure.** A structural enclosure must be constructed of masonry, heavy-gauge metal, or decay-resistant material that is also used with the architecture of the main building. The reviewing authority may allow materials other than those used for the main building if the finishes are similar in color and texture or if the proposed enclosure materials are more durable than those for the main structure. The walls must be sufficient to provide full screening from the affected roadway, pedestrian areas or adjacent properties. The enclosure may use overlapping walls to screen dumpsters and other materials (See Figure C.7.2.a) below).
- c. **Enclosure gates.** Gates must be made of heavy-gauge, site-obscuring material. Chain link or chain link with slats is not an acceptable material for enclosures or gates.
- d. **Collection areas.** Collection points must be located and configured so that the enclosure gate swing does not obstruct pedestrian or vehicular traffic or does not require that a hauling truck project into any public right-of-way. Ensure that screening elements allow for efficient service delivery and removal operations.
- e. **Landscape screening.** The sides and rear of service enclosures must be screened with landscaping at least five feet wide in locations visible from the street (except for alleys), parking lots, and pathways to soften views of the screening element and add visual interest.



Figure C.7.2.a. Both examples use durable and attractive enclosures with trees and shrubs to soften views of the enclosures from the side. The right example uses a trellis structure on top – a desirable example particularly where the top of the enclosures are visible from surrounding buildings, streets, and pathways (due to topography or building heights).

C.7.3. Location and screening of roof mounted mechanical equipment

- a. **Applicability.** This guideline applies to applicable developments that include roof mounted mechanical equipment and where the adverse effects of those elements cannot be adequately mitigated through location.
- b. **Rooftop equipment screening.** All rooftop mechanical equipment, with the exception of solar panels and roof-mounted wind turbines, including air conditioners, heaters, vents, and similar equipment must be fully screened from public view at the street level. Screening must be located so as not to interfere with operation of the equipment.

- c. Design of rooftop equipment screening.** All rooftop equipment screening devices must be well integrated into the architectural design through such elements as parapet walls, false roofs, roof wells, clerestories, or equipment rooms. Screening walls or unit-mounted screening is allowed but less desirable. The screening materials must be of material requiring minimal maintenance and must be as high as the equipment being screened. Wood must not be used for screens or enclosures. Louvered designs are acceptable if consistent with building design style. Perforated metal is not permitted.
- e.d. Location of rooftop equipment screening.** Locate and/or shield noise producing mechanical equipment such as fans, heat pumps, etc. to minimize sounds and reduce impacts at property lines of adjacent properties.

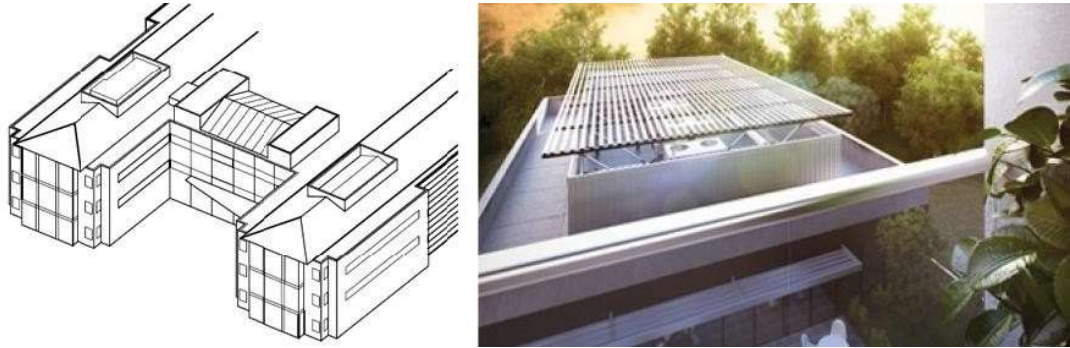


Figure C.7.3.a. Two examples of effective rooftop equipment screening. These images show how rooftop mechanical equipment can be located and screened effectively including side walls and a trellis to screen views from surrounding taller buildings.

C.7.4. Utility meters, electrical conduit, and other service utility apparatus

- a. Applicability.** This guideline applies to all residential developments covered by these Guidelines.
- b. Visibility of utility meters, electrical conduit, and other service utility apparatus.** These elements must be located and/or designed to minimize their visibility to the public and specifically not in the front setback or front of building. Project designers are strongly encouraged to coordinate with applicable service providers early in the design process to determine the best approach in meeting these standards. If such elements are mounted in a location visible from the street, pedestrian pathway, shared open space, or shared auto courtyards, they must be screened with vegetation and/or integrated into the building's architecture.



Figure C.7.4.a. Place utility meters in less visible locations. The left example successfully tucked the meters in a less visible location and/or screened by vegetation. The right example is poorly executed and would not be permitted in visible locations but may be acceptable on an alley or in a screened service area. Such meters must be coordinated and better integrated with the architecture of the building.

D. Site Design Element

The site design element guidelines address the elements located on the site other than primary buildings. Whereas Section C may address the location, size, and configuration of elements such as landscaping, lighting, walkways, etc., Section D addresses the quality and character of such elements.

D.1. Landscaping: Plant materials and screening

INTENT

- To encourage the abundant use of landscaping in site and development design to improve site aesthetics, enhance the pedestrian experience, and increase environmental quality.
- To reduce surface water runoff by percolating water through landscaped areas.
- To maintain and improve privacy for residential zones.
- To enhance buildings and open spaces.
- To make adjacent uses more compatible.
- To provide visual relief from roadways, parking areas, and the built environment.

GUIDELINES

D.1.1. Reference to Chapter 10.62 WCC Landscaping and Screening

- **Applicability.** This guideline applies to all residential development addressed in these guidelines.
- **Chapter 10.62 WCC.** The provisions of these guidelines are intended to supplement the standards found in WCC 10.62. If there is a conflict between the provisions of these guidelines and those of 10.62, the Director will determine those that apply. Exceptions to 10.62 WCC noted below apply.

D.1.2. Perimeter landscape buffer and screening

- **Applicability.** See WCC Chapter 10.62 for requirements for different zoning classifications. The Director may allow alternative solutions per WCC 10.62.050.

D.1.3. Parking lot landscaping

- **Applicability.** See WCC Chapter 10.62 for requirements for different zoning classifications. The Director may allow alternative solutions per WCC 10.62.050.

D.1.4. Landscaping of open spaces and yards

- **Applicability.** See WCC Chapter 10.62 for requirements for different zoning classifications. The Director may allow alternative solutions per WCC 10.62.050.

D.2. The design of walkways, pathways, and hardscape elements

INTENT

- To provide attractive internal pedestrian routes, promote walking, and enhance the character of the area.
- To provide safe and direct pedestrian access that accommodates pedestrians of all ages and abilities, minimizes conflicts between pedestrians and vehicular traffic, and provides pedestrian connections to neighborhoods.
- To accommodate bicyclists.



Figure D.2. Well-designed pedestrian ways are important in residential development.

GUIDELINES

D.2.1. Accessibility

- Applicability.** This guideline applies to all pathways and open spaces that are required as part of residential development by these guidelines.
- Walkway widths.** Walkways in developments (not within the public right of way) shall be sufficiently wide to serve their intended purpose and level of use. All required walkways shall be clear of permanent obstructions at the dimensions described below.
 - All walkways and pathways shall be at least five feet wide with widened areas for two wheelchairs to pass, except for walkways for very occasional use such as paths to a service area serving a single residential unit.
 - Walkways to main entrances to commercial businesses in mixed-use and live-work buildings shall be at least six feet wide.
 - Walkways where bicycle traffic is anticipated shall be at least 10 feet wide with two feet wide shoulders, signed, and configured for safe pedestrian traffic as well as bicycle movement.

- ~~c. — **Walkway safety.** Provide clear visibility along the walkway in developments (outside the public right of way). For safety and wayfinding, landscaping shall not block visibility to and from a path in a way that conflicts with the guidelines in Section C.5. Site Planning for Security. Maximize visibility where paths cross vehicle routes such as alleys and driveways so that pedestrians and drivers can clearly see each other.~~
- ~~d. — **Walkway enhancements.** Where walkways in developments (outside the public right of way) are within five feet of a “blank wall” measured perpendicularly to the wall (see Section E.7), at least one of the following measures must be incorporated:
 - ~~i. — Blank wall treatment per Section E.7, or~~
 - ~~ii. — A landscaped strip, at least three feet wide between the wall and the walkway featuring shrubs, trees, and/or vines as approved by the Director. The landscaping shall meet the standards of WCC 10.62 Landscaping.~~~~
- ~~e. — **Separation from the front facades of residential units.** See C.1.1. Ground related units facing streets, common pathways, or common open spaces.~~
- ~~f. — **Paving.** The paving of walkways from a residential entry to another residence or to a public ROW must comply with universal accessibility needs. Such pavements must be concrete, unit pavers, or material meeting the applicable accessibility standards.~~

D.3. Residential open space elements

INTENT

- To provide the amenities and features that make the required common space usable for its intended purpose.
- To provide residents of multi-family units, cottage housing, bungalow courts, and courtyard apartments a comfortable opportunity to socialize.
- To increase the livability of the residences in the development.

GUIDELINES

D.3.1. Landscape features and equipment

- Applicability.** This guideline applies to applicable residential development that includes common open space.
- Ground plane features.** Provide lawn area, plantings, and/or pavements that are conducive to residents' activities. This may include a broad range of activities from relaxing on seating, picnicking, low intensity informal play on a lawn, gardening, children's playground play, or active sports such as basketball. Large areas (six square feet or more) of bare earth or gravel are not permitted as part of the common open space unless they are part of a specific use such as a garden or sports field.
- Equipment.** Play equipment and similar features must be solid, durable, and designed not to present a hazard.
- Natural features.** Natural features such as trees, native vegetation, or water bodies may be incorporated into the common open space as long as they enhance the experience of people using the space (e.g.: if they provide an attractive element for viewing.)



Figure D.3.1.a. Example of natural element that may be considered as part of a required residential open space because the trail provides a distinct amenity for residents.

- Lighting.** See D.4 below.

D.4. Lighting

INTENT

- To encourage the use of lighting as an integral design component to enhance buildings, landscaping, or other site features.
- To increase personal safety and security.
- To increase night sky visibility and reduce the general illumination of the sky.
- To reduce horizontal light glare and vertical light trespass from a development onto adjacent parcels and natural features.
- To increase design consistency within the development.

GUIDELINES

D.4.1. Lighting levels and shielding

- Applicability.** This guideline applies to applicable housing types addressed in these guidelines.
- Site lighting levels.** All publicly accessible areas shall be lighted with levels as follows:

Table D.4.1. Site lighting levels.

Condition	Lighting levels in foot-candles on the ground		
	Minimum	Maximum	Preferred
Low or non-pedestrian and vehicular traffic areas, private parking lots, secure storage areas, etc.	.5	1.5	.5
Moderate pedestrian areas and building entries, most walkways, some pedestrian-oriented open spaces, etc.	1	4	2
High pedestrian areas such as building entries and areas where personal security is a concern	2	4	3
Public parking lots	.5	1.5	.5

The maximum exterior light level anywhere is four foot candles, measured on the ground. Lighting shall be provided at consistent levels, with an average lighting level to minimum lighting level uniformity ratio no less than 3:1, to create gradual transitions between varying levels of lighting and between lit areas and unlit areas. Highly contrasting pools of light and dark areas shall be avoided.

- Light quality, height, and shielding.** Adhere to the following unless there is a compelling reason to the contrary:
 - Parking area lighting fixtures shall be fully shielded; dark sky rated and

mounted no more than 15 feet high with lower fixtures preferable to maintain a human scale.

- ii. Exterior lighting shall not spill onto adjacent properties.
- iii. Ground mounted lighting shall have a maximum height of 15 feet.

D.5. Fences, walls, and vertical elements

INTENT

- To ensure that site features such as walls, fences, poles, gates, and screens are well constructed and easily maintainable.
- To enhance the pedestrian environment and the general appearance of residential neighborhoods.

GUIDELINES

D.5.1. Requirements for fences and walls

- Applicability.** This guideline applies to applicable housing types covered by these guidelines.
- General fence standards.** Meet the requirements of WCC 10.48.130 unless otherwise directed below.
- Visibility.** Except where full screening of service areas, utilities, mechanical equipment, or other facilities is necessary, fences shall not obscure building fronts, or attractive site features, or sight triangles needed for safety in accordance with WCC 10.48.130. See also Section C.5.
- Fences and walls shall be made of durable,** easily maintainable, and vandal-resistant materials. To reduce the likelihood of graffiti, avoid walls with smooth surfaces unless they can easily be repainted or cleaned.
- Chain link fences are discouraged** but allowed along the street facing fronts of residential development except for multifamily development. Chain link fences are prohibited in street front setbacks of multifamily residences. Chain link fences may be used for temporary purposes such as construction or short-term security.



Figure D.5.1.a. The fences on the top row are appropriate in residential zones. Those on the bottom left two examples are not acceptable within street front setbacks. However, they may be acceptable alongside and rear yard setbacks.

- g. Where a retaining wall adjacent to a public sidewalk** is necessary for development, step the wall back so that the top of the wall surface adjacent to the sidewalk is no more than 30 inches high. If greater height is required, construct terraces or slopes that do not exceed 30 inches vertical to 18 inches horizontal (5 vertical to 3 horizontal). See Figures D.5.1.b and D.5.1.c. The Director may allow other configurations, such as a higher wall set back from the sidewalk and landscaped, provided such a wall is necessary for development and the objectives of this section are achieved.

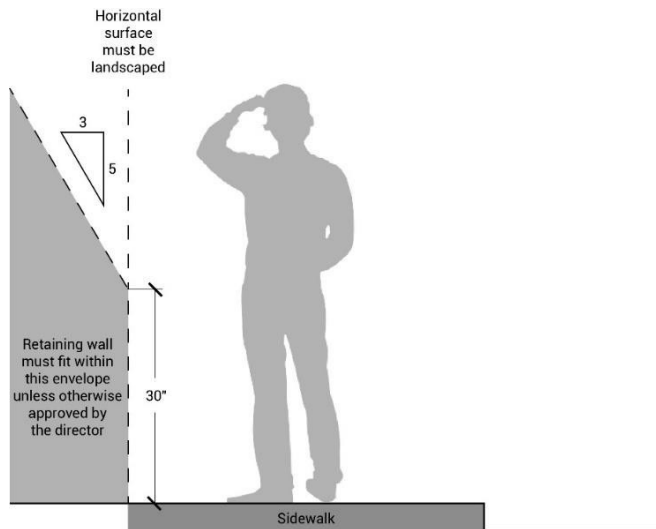


Figure D.5.1.b. Allowed envelope for walls in street front setback.

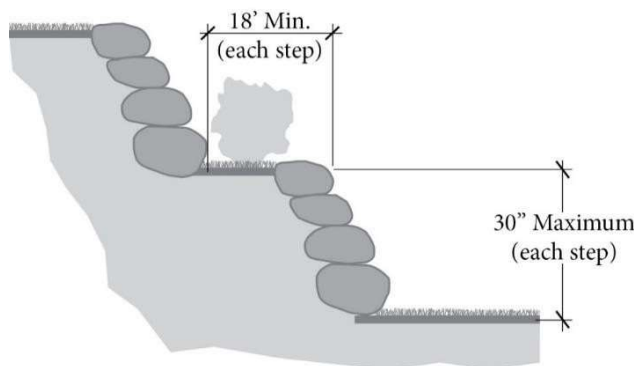


Figure D.5.1.c. Allowable terrace configuration for wall or rockery adjacent to a public sidewalk. Note that this illustration shows a stone wall, but other masonry and concrete walls are acceptable as well.



Figure D.5.1.d. Examples of appropriate measures to handle grade change in street front setbacks.

D.5.2. Poles and other vertical elements outside of the public right of way.

(Note: This guideline does not apply to desirable vertical elements intended to enhance the pedestrian environment such as weather protection, flag poles, banners, supports for hanging flower baskets, trellises, and supports for green walls.)

- a. Minimize vertical site elements on property outside of a public right of way except for those intended specifically to add to the pedestrian environment.
- b. Wires and other hanging features are prohibited except for otherwise allowed temporary features such as permitted signs.
- c. See also guidelines for service areas and mechanical equipment, Section C.7.

E. Building Design

E.1. General

Many of these building design guidelines call for a building to feature one or more elements from a menu of items. In these cases, a single element, feature, or detail may satisfy multiple objectives. For example, a specially designed or fabricated covered entry with attractive detailing might be counted toward requirements for architectural character, human scale, building corners, and building details.

The terms “decorative” and “ornamental” are not necessarily meant to mean “characterized by traditional patterns, nonstructural elements, or applied markings.” Elements may be considered “decorative,” “ornamental,” or “special” if they extend beyond the typical level of quality, use materials or forms in an unusual way, or show special architectural consideration. The Director shall determine what elements are “ornamental,” “decorative,” or “special.”

E.2. Architectural character

INTENT

- To reinforce a distinct building design character for Wenatchee’s neighborhoods.
- To create more design continuity and compatibility between nearby building facades.
- To integrate new residential building types into Wenatchee’s design character.
- To create attractive and functional streetscapes.
- To allow for creativity in the design of new buildings.

GUIDELINES

E.2.1. Architectural character of different residential areas

- Applicability.** This guideline applies to applicable housing types covered by these guidelines.
- Rationale and objectives.** As noted in Section B.2, one of the objectives of these guidelines is to reinforce the architectural design character of different neighborhoods or areas within the City. Although buildings in all of Wenatchee’s neighborhoods feature different architectural styles and characteristics, there are subtle differences between different areas in the City, generally as a result of the period of construction, topography, environmental context, and economic factors.

For example, newer residential districts in the Perimeter Residential Neighborhoods Character Area often feature buildings with prominent garages, more picture windows, a more horizontal configuration, smaller porches, more complex rooflines, and in the case of multi-family construction, fewer details and more contemporary building materials. Appreciating and reinforcing such characteristics can, if frequently employed by property owners, give a neighborhood the unique identity that is desired. While the vast majority of buildings in Wenatchee’s residential zones are single family houses, they provide a context for new, sometimes larger buildings accommodating a wider variety of dwelling types. Therefore, if the new buildings can feature some of the local area’s architectural features, they will likely fit better into the neighborhood and reinforce its neighborhood character.

The intent of this guideline is not to apply a strict set of standards to produce a homogenous neighborhood with little variety. Therefore, the applicant is given a variety of options to address the requirement.

- c. Character area specific architectural design requirements.** In the design of new residential buildings, incorporate elements and characteristics relevant to the applicable character area defined in Section B.2. Table E.2.1.c identifies those architectural design characteristics that are typical of the individual character areas. Specifically:
- i. The applicant shall demonstrate to the Director's satisfaction that the project incorporates the architectural design elements and characteristics indicated in the chart.
 - ii. If there is a disagreement between the Director and the applicant regarding the adherence to this section, the matter will be referred to the Wenatchee Planning Commission for an informal design review workshop. The recommendation of the Planning Commission to the Director on the specific topic shall be incorporated into building plan revisions and resubmitted to the City for final review. The ultimate decision to approve, approve with conditions, or deny the permit remains with the Director.

Requirements by character area:

- i. Development in the Core and Perimeter Neighborhoods Character Area must incorporate at least four of the elements noted by dots in that column.
- ii. Those in the Sunnyslope Character Areas must incorporate at least two of the elements indicated.
- iii. Development in the Greater Downtown, North/South Wenatchee Business Districts, and Waterfront must incorporate at least three of the indicated elements if the project is a new building.


The chart emphasizes that the applicant may propose other architectural design measures to meet one of the minimum required elements (see Table E.2.1.c for details). The project may also include other architectural design features in addition to those required by this guideline. Nothing in this guideline is intended to limit the creativity or uniqueness of an architectural design proposal, so long as it does include the measures previously noted to enhance the local neighborhood as a whole.

TYPICAL CHARACTER AREA ARCHITECTURAL ELEMENTS



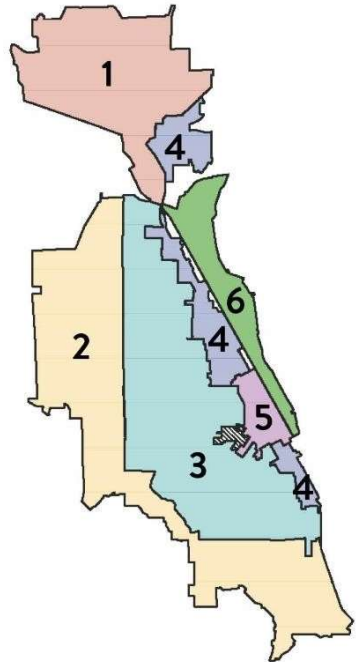

2. PERIMETER RESIDENTIAL NEIGHBORHOODS

- Wide street frontage
- Multiple gables
- Small entry nook
- Mixture of vertical & picture windows
- Multiple facade materials




3. CORE RESIDENTIAL NEIGHBORHOODS

- Usable porches
- Simple roofs
- Vertical windows with details
- Traditional materials


1. SUNNYSLOPE

- Pitched roof
- Usable porch
- Vertical windows
- Traditional materials




6. WATERFRONT

- Ornamental materials or details
- Modulation of large facades



4. NWBD & SWBD

- Wide mix of building types
- Building details are most important



5. GREATER DOWNTOWN

- Mix of window types
- Enhanced entry & window details
- Traditional & decorative materials

Figure E.2.1.a. Typical architectural characteristics of different character areas.

Table E.2.1.c. Architectural design characteristics that are typical of the individual character areas.

Character Giving Architectural Elements	Character Areas					
	1. Sunnyslope	2. Perimeter Residential Neighborhoods	3. Core Residential Neighborhoods	4. North and South Wenatchee Business Districts	5. Greater Downtown District	6. Waterfront
Narrow (less than 12') or no garage			●			
Multiple gables		●	●			
Horizontal building form (building wider than tall)	●	●				
Pitched roof	●	●	●			
Porch large enough for resting	●		●			
Picture windows	●	●		●	●	●
Vertical windows	●	●	●	●	●	●
Window and door details		●	●	●	●	●
Ornamental materials or details				●	●	●
Traditional building materials (brick, wood siding, wood shingles)	●	●	●	●	●	●
Wildland/Urban Interface non-flammable building materials	●	●				
Accentuated entry in a large building				●	●	●
Modulation of large facades		●	●			●
Other (as approved by the Director)*	●	●	●	●	●	●
Number of characteristics a project must include	2	4	4	3	3	3

* The applicant may propose other architectural design measures (for one of the minimum required elements). These may be features that are common in nearby residences (by providing a rationale with illustrative photos of nearby lots to the City) or maybe other design

features that demonstrably meet the guideline's intent. The Director will determine whether or not the proposal is acceptable.

- d. **Exception: Renovations to existing buildings in the Greater Downtown.**
Exterior renovations and additions to existing buildings do not need to meet the requirements of Section E.2.

E.3. Architectural composition, massing, and articulation

INTENT

- To employ facade articulation techniques to reduce the perceived scale of large buildings and add visual interest from all observable scales.
- To create a neighborhood-oriented streetscape that is visually interesting.
- To create clear and welcoming building entries.
- To encourage development that is compatible with buildings on the same block in terms of “architectural scale.” Architectural scale is the perceived height and bulk of a building relative to that of neighboring buildings.
- To add visual interest to buildings.

DEFINITIONS

- **Architectural scale** is the perceived height and bulk of a building relative to that of neighboring buildings. A building has “good architectural scale” if its visual size is relatively similar to its neighbors.
- **Modulation** is a stepping back or projecting forward of portions of a building face, within specified intervals of building width and depth, as a means of breaking up the apparent bulk of a structure’s continuous exterior walls. (Ref: WCC 10.08.100.M). “Vertical modulation” means that the stepping or projecting is vertical. (See Figure E.3.1.b, left picture.)
- **Articulation** is visually breaking up a building facade into intervals by including repetitive features, such as broken rooflines, chimneys, entrances, distinctive window patterns, street trees, and modulation.

GUIDELINES

E.3.1. Scale, compatibility, and facade variety

- Applicability.** This guideline applies to applicable housing types covered by these guidelines **when there are five or more units per legal lot of record**. Some requirements only apply to specific residential building types as noted. These standards apply to all facades facing a public or private street, courtyard, or common open space. These standards also apply to all facades facing adjacent properties and which are two stories in height or greater.
- Facade articulation intervals for residential buildings and residential portions of mixed-use buildings.** Residential portions of new buildings shall include articulation features at intervals no greater than indicated for individual house types as follows to break up the massing of the building and add visual interest and compatibility to the surrounding context. Also noted are special requirements for facade composition for applicable types.
 - Cottage housing.** Not required.

- ii. **Duplex, Triplex, and Fourplex.** 30 feet maximum width without articulation. Note: this may be accomplished by setting back a garage or by an extended porch or entry platform. *To prevent monotonous streetscapes, the same building street front elevation design for these house types shall not be utilized on consecutive units or lots, except upon demonstration to the director that the alteration of building facades, materials, and/or methods would provide comparable visual diversity and individual identity to the structures as different building elevation plans. Two or more structures that are located close enough to each other to clearly compare their similarities, that have substantially similar front facades, shall incorporate at least two different design methods from structure to structure that achieve visual diversity.* Methods which may be utilized to achieve visual diversity include, but are not limited to, differing siding material or style, building modulations, and roof line variations.



Figure E.3.1.a. This attractive duplex is in one of Wenatchee's predominantly single family neighborhoods and articulates a simple rectangular floor plan with porches, window patterns, and eyebrow windows.



Figure E.3.1.b. Two examples of modulation effectively used on a duplex (left) and a triplex (right).

- iii. **Townhouses.** The maximum facade width without articulation shall be no greater than three-fourths (3/4) of the width of the unit or 15 feet, whichever is greater. The width of the unit is the distance between a shared wall and an exterior or shared wall. The measurement is taken perpendicular to a shared wall and may be at the widest point of the unit. All applicable facade(s) of each unit must feature some articulation as illustrated in Figure 3.1.c, below.

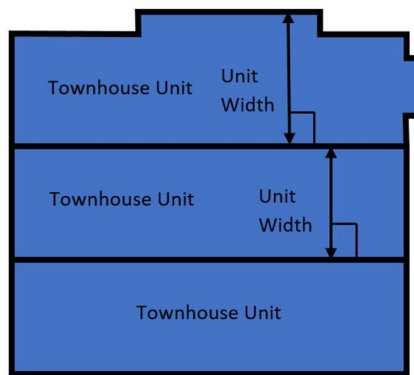


Figure E.3.1.c. The units in the top left image each have distinct, but identical windows and roof forms. The outside unit on the right is differentiated through the use of building materials, window design, unit size, and facade detailing. The units in the top right example include reverse elevations that add to the building's composition. The image on the bottom left illustrates how the width of the unit is measured.

- iv. **Courtyard multifamily.** For facades facing the courtyard or internal pathway, the maximum facade width without articulation shall be no greater than the width of the unit. For facades facing the street, the maximum width without articulation is 30 feet.



Figure E.3.1.d. Four examples of articulation in courtyard complexes. Some use simple gables only while others are enhanced with building elements. The example on the lower right incorporates an asymmetrical arrangement of building forms to add interest to an otherwise uniform architectural scheme.

- v. **Multi-family.** The maximum facade width without articulation shall be no greater than the width of the unit.



Figure E.3.1.e. Examples of articulation of multifamily residential building facades. All examples use window fenestration and vertical building modulation elements. The upper examples use a mixture of material changes and detail components to further articulate the facades. In the lower left example, the balcony element's dramatic mix of color, material, and projection are effective in meeting the standards. The lower right example adds a combination of materials, color changes, and building modulation to help meet the following standards.

- vi. **Live-work residences.** The maximum facade width without articulation shall be no greater than the width of the unit.

vii. Mixed-use with residential on upper stories. The maximum facade width without articulation shall be no greater than the width of the unit.

- c. Facade articulation methods.** At least three of the following methods shall be used at intervals previously indicated. Methods that apply to a facade shall apply to the full height or appear on each floor as appropriate. Facades that face public or private streets, shall utilize method iii as one of the three choices and a second method must be selected from i, iv, and v.
- i. Use of window patterns and/or entries. Window and/or entry patterns create a repeating vertical set of windows across stories of the structure as illustrated in Figure E.3.1.f.



Figure E.3.1.f. This handsome house in a Core Residential Character Area uses window patterns and planter boxes as well as an entry feature to articulate its facade, rather than modulation and material changes.

- ii. Change in building material, siding style, and/or window fenestration pattern.
- iii. Vertical building modulation of at least three feet in depth (extending out or stepping back from the building face), or two feet if the modulation is accompanied by roof modulation or change in materials. Balconies may be used to qualify for this option if they are recessed or projected from the facade by at least two feet. “Juliet balconies” (balconies that are so shallow that they cannot be occupied) or other balconies that appear to be tacked onto the facade will not qualify. The depth of the modulation must be at least three feet if not accompanied by roof modulation or a change in materials.



Figure E.3.1.g. Examples of acceptable vertical modulation. The example on the left features the stepping back of facade elements as well as different materials, colors and rooflines. In the example on the right, the balconies that have been integrated into the architecture of the building.

- iv. Roofline modulation. In order to qualify as a facade articulation feature, rooflines shall employ one or more of the following:
 - a. Different roof heights. For flat roofs or facades with horizontal eave, fascia, or parapet, the minimum difference between the two sections of roofline or cornice is three feet for one and two story buildings and five feet for buildings with more than two stories.
 - b. A pitched roofline or gabled roofline segment. The pitched or gable roof shall be at least 20 feet in width with a minimum slope of 4:12.
 - c. An arched roof line or a roof with special geometry or an unusual or distinctive roof element such as an arch, tower, clock, or similar feature as an articulating feature.



Figure E.3.1.h. Four examples meeting the criteria for roofline modulation.

- v. Distinctive building elements such as bay windows, balconies, and weather protection over windows.
- vi. Prominent and “structural” landscaping such as evergreen trees or landscaping that varies according to the articulation intervals listed above such as individual gardens or courtyards situated along the front building facade. The methods employed must effectively break up the massing at no more than the intervals previously stated and effectively treat the full vertical height of the applicable facade, as determined by the director.

E.4. Pedestrian-oriented facades for mixed-use buildings and transparency for residential buildings

INTENT

- Provide a consistently interesting pedestrian environment.
- Strengthen retail activities in Downtown Wenatchee.
- Allow pedestrians to view the inside activities of businesses and associated non-residential uses.
- Provide accessible non-residential ground floor space that is adaptable to a variety of uses.
- Increase safety by adding “eyes on the street or open space”.

GUIDELINES

E.4.1. Pedestrian-oriented facades

- a. Applicability.** This guideline applies to buildings where “pedestrian-oriented facades” are required in Section C.1.2.
- b. Where pedestrian-oriented facades are required,** the building shall meet all of the following:
 - i. Transparent window areas or window displays over at least 75 percent of the ground floor facade between two feet and eight feet above the sidewalk. The windows shall look into the building’s interior or be configured as merchandise display windows. The building must be designed so that the windows satisfying the requirement for “pedestrian-oriented facades” do not look into service or storage areas or other unsightly rooms.

Special purpose buildings such as theaters that do not sell merchandise may propose an alternate solution to providing an interesting pedestrian experience. Offices, banks, and restaurants do not qualify for this exception because they are uses that can provide the required transparency without compromising their functions. If the applicant’s proposal includes windows that are not fully transparent to satisfy this requirement, the alternative materials must be approved by the Director.
 - iii. A primary building entry facing the street front. (See Section E.5 for entry enhancement requirements.)
 - iv. Weather protection (e.g. canopy, awning, or other cover from the rain) at least five feet wide over at least 65 percent of the front facade. The weather protection must be located between eight feet and 15 feet above grade unless the Director determines there is a compelling reason to the contrary.
 - v. A floor to floor ground floor height of at least 15 feet. The Director may waive this requirement if there are special conditions such as the provision of a mezzanine.

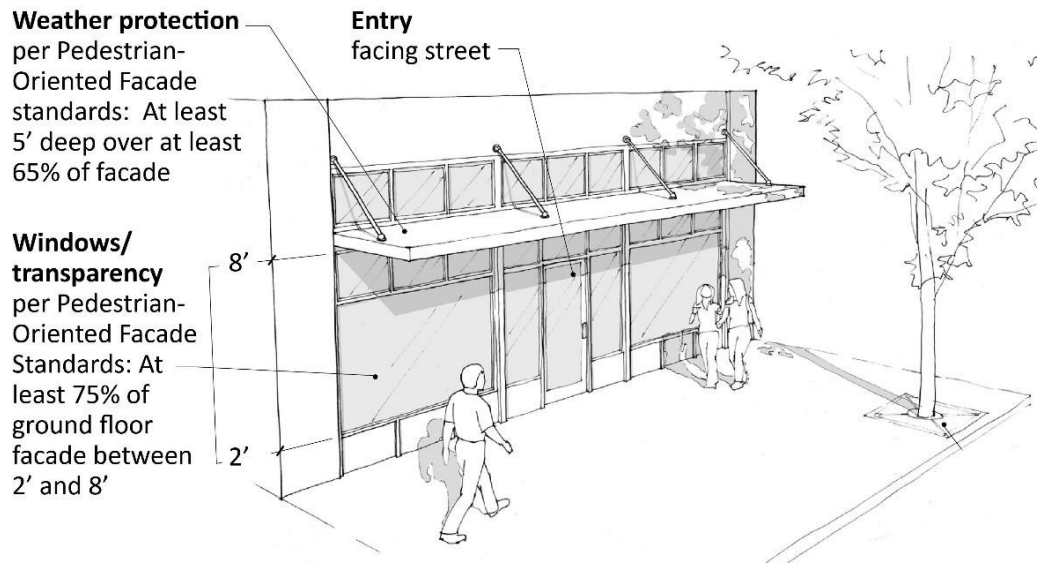


Figure E.4.1. Pedestrian-oriented facade.

E.4.2. Transparency for residential buildings facing the street

- a. **Applicability.** This guideline applies to applicable housing types covered by these guidelines.
- b. **Transparency.** All residential building facades directly facing a public street, common open space, or common pathway shall feature at least the minimum amount of clear window area as a percentage of the total facade area. Note: windows in doors and on balconies may be counted.
 - i. For ground floor residential units facing the public or private street (except an alley), common open space, or pathway: at least 15 percent of the street facing facade.
 - ii. For living spaces on upper stories, at least 15 percent of the facade.
 - iii. Where the units face a common open space or pathway rather than the street, the facade facing the street must feature at least 10 percent transparency.

E.5. Building details: Entrances, windows and corners

INTENT

- To provide attractive and welcoming building facades and pedestrian environments.
- To increase the design quality of buildings when viewed close up (roughly within 80 feet of the building.)
- To enhance the quality of building facades.
- To provide the building with an appropriate human scale that adds interest and a sense of well-crafted quality.
- To add interest to the streetscape for those travelling in vehicles, bicycles, and on foot.

GUIDELINES

E.5.1. Building entrances: Configuration and details

- a. **Applicability.** This guideline applies to all housing types covered by these guidelines when there are five or more units per lot.
- b. **Building entries.** Building entries must adhere to all of the following:
 - i. All buildings shall have a primary building entrance facing the street or onto a courtyard or pathway directly leading to the street except as follows:
 - Townhouse units with a facade that is facing and visible from a public or private street shall have the primary entrance oriented to the street and accessed by a walkway directly leading to the street.
 - Buildings with a shared entrance leading to a shared interior hall and that have a facade facing the street shall have a building entrance facing the street and accessed by a walkway directly leading to the street.
 - ii. All primary building entries shall be covered with weather protection at least six feet wide and five feet deep if they serve more than one residence or at least four feet wide by three feet deep if they serve a single residence.
 - iii. All primary building entries shall feature lighting for visibility and security.
 - iv. All building entrances servicing more than one residence shall feature at least one of the following:
 - Decorative or special use of a material such as colored tile or special wood molding.
 - Decorative weather protection element.
 - Etched or stained decorative glass.
 - A hand-crafted sign with the name of the building.
 - Artwork.
 - A planter area.
 - Decorative entry door(s).
 - Other decorative or specially designed entry treatment that the Director determines meets the intent of this section.



Figure E.5.1.a. Examples of attractive entry details including decorative weather protection features, special lighting, interesting window patterns, and special signage.

E.5.2. Window details

- a. **Applicability.** This guideline applies to all housing types covered by these guidelines when there are five or more units per lot.
- b. **All residences shall feature** at least two of the following measures to add quality and interest to fenestration on building facades:
 - i. Recess or project individual windows at least two inches from the facade or incorporate other design treatments that add depth, richness, and visual interest to the facade.
 - ii. Arrange the grouping of windows in a distinctive pattern.
 - iii. Incorporate multi-paned windows (more than four panes per window).
 - iv. Employ visually significant window trim or shading elements such as four inches or wider frames, lintels, or casings.
 - v. Other method approved by the Director that meets the intent of these guidelines.



Figure E.5.1.b. Examples of window details. The two on the left employ trim, multiple panes and window groupings. The one on the right provides no refinement or sense of quality.

E.5.3. Corner elements

- a. **Applicability.** This guideline applies to multistory mixed-use and multi-family buildings over three stories that are on designated “high visibility corners” indicated in Figure E.5.3.b. (This guideline does not apply to duplexes, triplexes, fourplexes, townhouses, or courtyard apartments.)
- b. **All applicable buildings shall have at least one corner design feature** approved by the Director that accentuate the street corner. Corner design features can include a cropped building corner with a prominent entry, decorative use of building materials at the corner, distinctive facade articulation, sculptural architectural element, or other decorative element the Director determines meets the intent of the standards. Alternatively, the building can be configured with a corner plaza.



Figure E.5.3.a. Examples of acceptable high visibility corner treatments. The example on the right breaks down the building massing to a human scale and adds a corner entrance and graceful canopy.

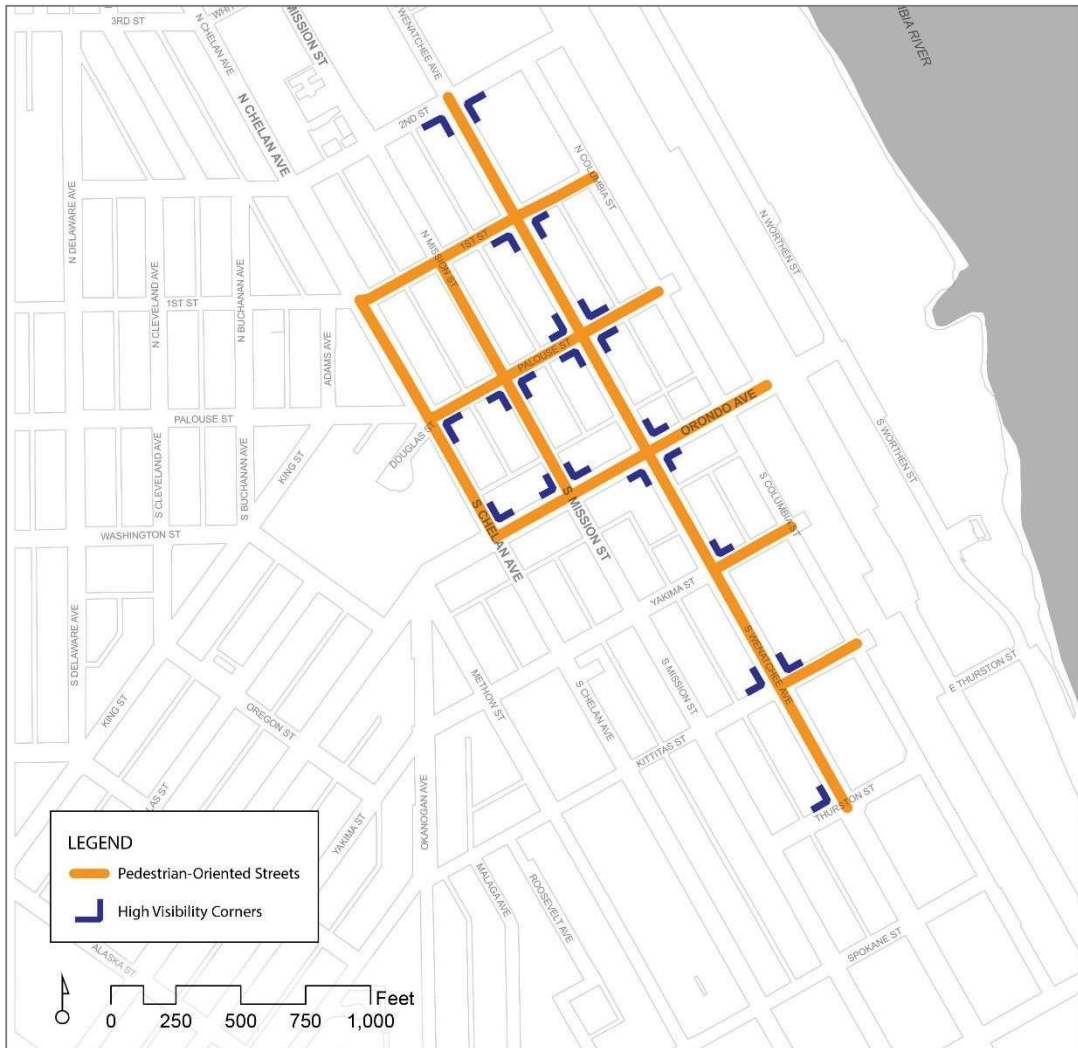


Figure E.5.3.b. Designated high visibility corners.

E.6. Building facade materials

INTENT

- Encourage the use of durable, high quality, and urban building materials that minimize maintenance cost and provide visual interest from all observable vantage points.
- Promote the use of a distinctive mix of materials that helps to articulate facades and lends a sense of depth and richness to the buildings.
- Place the highest priority for the quality and detailing of materials on the first two to three building floors.

GUIDELINES

E.6.1. Conditions for the use of specific building materials

- Applicability.** This guideline applies to all residential buildings, when there are five or more dwelling units on one lot and to all mixed-use buildings ~~except single-family residences and cottage housing~~. Where the provisions herein conflict with building materials standards in WCC Title 10, the provisions herein shall apply. This guideline shall also apply to structures and portions of structures that are permitted as residential shared general storage.
- Conditions for concrete only units (concrete block).** Concrete block (a.k.a. concrete masonry unit CMU) may be used as indicated in the permitted materials chart in Section E.6.2 provided special enhancements such as tile, special finishes, and colors or some other approach approved by the Director are included to add a combination of textures and/or colors.



Figure E.6.1.b. The left facade illustrates an acceptable alternative example, as CMU is used as the primary cladding material. Note the use of split-facade CMU's above each of the awnings coupled with the use of smooth-facade CMU's on the vertical columns (which employ black accent tiles for added interest.). The right facade uses smooth-faced CMU for the main entry to function as a contrast to the brick (primary facade material) and to highlight the entry details (canopy, lighting, and doors).

- Conditions for metal siding.** Metal siding may be used as noted in Permitted Materials Chart if it is incorporated with other permitted materials and complies with the following:
 - i. It features visible corner molding and trim and does not extend to the ground level of mixed-use buildings and extends no lower than two feet above grade

for residential buildings. Masonry, concrete, or other durable material shall be incorporated between the metal siding and ground plane.

- ii. Metal siding shall be factory finished with a matte, non-reflective surface.
- iii. Where metal siding is the primary or secondary siding material on upper floors, the layout of the panels must be coordinated with the location and patterns of windows, balconies, and modulated building surfaces to provide an integrated appearance.



Figure E.6.1.c. Metal siding used appropriately in combination with other materials. The use of metal siding in each example above is secondary to masonry. The left and right images are more contemporary in character, whereas the middle image is more rustic and industrial, with more refined windows.

d. Conditions for fiber cement panel or plank systems (e.g.: Hardie Panel systems).

- i. Fiber Cement board, panel, or plank siding products (e.g. Hardie Panel and Plank) may be used where indicated in the permitted materials chart in this section, provided:
 - Where fiber cement panel or plank siding is the primary or secondary material, the paneling joints must be arranged in a pattern that is consistent with windows, balconies, and modulated building surfaces and must be enhanced with facade details that add visual interest from the ground level and adjacent buildings.
 - Fiber Cement panel or plank may not be used on ground level facades containing non-residential uses.
 - Where fiber cement panel or plank is the dominant siding material, the design must integrate a mix of colors and/or textures that are articulated consistent with windows, balconies, and modulated building surfaces and are balanced with facade details that add visual interest from the ground level and adjacent buildings.



The above building uses fiber cement panels and planks in different textures and colors to help articulate the facade. The white color replicates the board and batten style in the left image and green color in the right image effectively replicates horizontal wood siding.



The fiber cement panels covering a large area in a single color would not meet the purpose of the standards. The right image is a better example and combines larger panels (dark maroon color) with horizontal planks (beige color) as effective articulation features.

Figure E.6.1.d. Acceptable and unacceptable (lower left) use of panel board systems.

- e. Conditions for Exterior Insulation and Finish System (EIFS).** EIFS may be used as noted in Permitted Materials Chart if it meets the following conditions:
- i. EIFS is prohibited within 2 vertical feet of the sidewalk or ground level or in areas that are especially subject to deterioration from human contact such as around a primary building entry or front facade adjacent to a sidewalk. Concrete, masonry, or other highly durable material(s) must be used for the subject ground level building elevations to provide a durable surface where damage is most likely.
 - ii. EIFS must feature a smooth or sand finish only.
 - iii. EIFS must be trimmed in wood, masonry, or other material and must be sheltered from weather by roof overhangs or other methods
 - iv. Applicants proposing to use EIFS as an exterior building material, must submit a manufacturer's product description and warranty to the Director for approval.



Figure E.6.1.e. Examples of acceptable and unacceptable use of EIFS. Buildings 1 and 2 mix EIFS with brick and other materials and integrate trim details around windows to add a sense of depth to the facade. Building 3 uses EIFS in between the window and sidewalk - this design is prohibited. Building 4 uses EIFS as the primary siding material, which is prohibited except in the North and South Wenatchee Business District areas.

f. Conditions for wood panels and similar products.

- i. Wood panels must be finished to avoid deterioration and be separated (raised above the ground at least 6"
- ii. Panel edges must be trimmed and the top of the panels protected from the weather. No unfinished or untrimmed edges are allowed.
- iii. T-111 paneling and similar products are not permitted.

g. Conditions for ceramic tile, quarry tile and similar materials.

- i. Tile and ceramic materials must be exterior grade and freeze resistant.
- ii. Tile and ceramic materials must be firmly grouted with exterior grade materials.

h. Conditions for concrete construction.

- i. Concrete finishes must be either smooth and featureless or purposefully textured with a consistent pattern.

i. Conditions for stucco, stucco-like and similar troweled finishes.

- i. To avoid deterioration, the finish material must be trimmed and/or sheltered from extreme weather by roof overhangs or other methods.
- ii. The finish material may only be used in conjunction with other approved building materials.
- iii. Heavily troweled markings and randomly implanted rocks into the stucco are not allowed.

E.6.2. Building materials allowed in specific character areas.

- a. **Applicability.** This guideline applies to all residential buildings, when there are five or more dwelling units on one lot and to all mixed-use buildings, ~~except single family residences and cottage housing.~~ This guideline shall also apply to structures and portions of structures that are permitted as residential shared general storage.
- b. **Character area and specific materials requirements.** The following chart identifies where materials are permitted in each of the character areas identified in Section B.2 according to the following legend/markings:


P = Permitted as a primary, secondary, or accent material.

S = Permitted as a secondary or accent material.


A = Permitted as an accent material.

N = Not permitted.

C = The material may be permitted, but the Director may require added conditions in order to ensure the durability and quality of materials provide a high level of design, construction, finish, and increase consistency with the desired design character for the particular Character Area described in B.2. See Section E.6.1.

 The color green indicates the material is generally acceptable in the character area.

 Yellow indicates there are some conditions or limitations for using the material.

 Red indicates the material is not acceptable for that character area.

Primary material means any single material that covers more than 35 percent of the facade.

Secondary material means any single material that covers 35 percent or less of the facade.

Accent material means any single material used for a small building element such as a door, canopy, weather protection, light fixtures cornice, trim (e.g. window trim), signs, or artwork.

Other materials not covered in this section will be evaluated by the Director based on the intent statement of this section. The Director may allow materials otherwise prohibited if the applicant can demonstrate to the Director's satisfaction that the material is durable, appropriate for the proposed purpose and application, and meets the objectives of this section.

Table E.6.2. Permitted materials chart

Materials	Core Res. - bottom floor	Core Res. - above first floor	Perimeter Residential	Sunnyslope	N/S Wenatchee Business Dist.	Greater Downtown	Waterfront
Brick, stone, masonry except for CMU	P	P	P	P	P	P	P
CMU, Plain	N	N	N	S	SC	SC	SC
CMU with enhancements	SC	SC	SC	SC	P	P	P
EIFS	N	SC	SC	SC	PC	SC	SC
Metal siding	N	N	SC	SC	PC	PC	PC
Fiber cement panels (e.g.: Hardie Panels)	PC	PC	PC	SC	PC	PC	P
Fiber cement plank w/ pattern (e.g.: Hardie Plank)	P	P	P	P	P	PC	P
Lap siding, wood shingles or similar	P	P	P	P	S	S	P
Wood panels with special finish and texture	PC	PC	PC	PC	PC	PC	PC
Mirrored or highly reflective surfaces	N	N	N	N	AC	N	N
Plastic or sheet fiberglass	N	N	N	N	N	N	N
Ceramic tile and similar	A	A	A	A	S	S	S
Concrete	SC	SC	SC	SC	SC	SC	SC
Stucco	PC	PC	PC	PC	PC	PC	PC

E.7. Blank walls

For the purpose of this guideline, a “blank wall” is:

Any ground level wall surface or section of a wall over four feet in height at ground level that is longer than 15 feet as measured horizontally without having a ground level window or door lying wholly or in part within that 15 foot section.

INTENT

- To ensure that large expanses of uniform walls visible from a public roadway or park do not detract from the local neighborhood’s appearance or the pedestrian environment.

GUIDELINES

E.7.1. Blank wall treatments

- Applicability.** This guideline applies to applicable housing types covered by these guidelines. A “blank wall” is defined as:
 - A wall or portion of a wall that has 400 square feet of vertical surface without a window, door, building modulation, or other architectural feature, or
 - Any ground level wall surface or section of a wall over four feet in height at ground level that is longer than 15 feet measured horizontally without having a ground level window or door lying wholly or in part within that 15 foot section.
- New blank walls.** Untreated blank walls facing and within 10 feet of a public street, pedestrian-oriented space, common usable open space, or pedestrian pathway are prohibited unless treated as described as follows.

At least 40 percent of the vertical wall between two feet and ten feet above grade must be treated. A combination of the methods described below may be used to achieve the 40 percent. Acceptable methods to treat blank walls can include, but are not limited to:

- A vertical trellis in front of the wall with climbing vines or plant materials.
- Transparent windows, doors or other features that allow visibility to the interior of the building.
- Trees or shrubs between the building and pathway or open space.
- Building detailing that adds visual interest at a pedestrian scale. Such detailing shall use a variety of surfaces; monotonous designs will not meet the purpose of the standards.
- Display windows at least 16 inches deep to allow for changeable displays. “Tack-on” display cases shall not qualify as a blank wall treatment. (Applicable for mixed-use buildings only.)
- Artwork such as a mural or bas-relief sculpture.
- Other method approved by the Director.



Figure E.7.1. Acceptable blank wall treatments.

E.8. Requirements for above grade structured parking (multiple user parking garages)

INTENT

- To minimize negative visual impacts of parking garages or above grade structured parking in buildings.

GUIDELINES

E.8.1. Structured parking design

- Applicability.** This guideline applies to all above grade structured parking including above grade parking within buildings as well as stand-alone parking garages. Exception: Structured parking and parking garages screened from public roadways by a building (i.e. a building located between the garage and the public street) are not required to comply with these criteria.
- Above grade structured parking and parking garages must be designed** to obscure the view of parked cars at the ground level. This does not require opaque screening that would create security issues in conflict with Section C.5 Site Planning for Security.
- Where the structured parking or garage wall is built within 10 feet of the sidewalk edge,** the grade level facade shall incorporate a combination of artwork, grillwork, special building material, treatment, design, or other treatments as approved by the Director that enhance the pedestrian environment. Small setbacks with terraced landscaping elements can be effective in softening the appearance of a parking garage.
- Above grade structured parking must use articulation treatments** (not necessarily building modulation) that break up the massing of the parking structure and add visual interest as described in Section E.3.1.



Figure E.8.1.a. The side of this parking garage includes decorative grillwork and a raised brick planter to enhance the pedestrian environment.



Figure E.8.1.b. This building uses openings on its second level parking area to resemble windows.



Figure E.8.1.c. Design parking garages to obscure the view of parked cars. Note the landscaping that separates the garage from pedestrians.

Proposed 2024 annual amendments to the Wenatchee Urban Area Comprehensive Plan are to the residential portion of the Land Use Implementation Chart located in the Land Use Element on page 38 of 211. Proposed amendments appear on the next page of this document.

District	Primary Uses	General Development Standards
Residential Foothills Low	<p>Primary Uses:</p> <ul style="list-style-type: none"> ▪ Single family ▪ Duplex <u>Middle housing up to 3-units in a structure</u> <p>Secondary Uses:</p> <ul style="list-style-type: none"> ▪ Accessory Dwelling Units ▪ Agricultural uses ▪ Home based businesses 	<ul style="list-style-type: none"> ▪ Very low density residential suited for outlying areas taking into consideration topography and foothills aesthetics. ▪ Density: Up to 4 units per acre <u>or applicable middle housing unit density.</u> ▪ Height: 30ft
Residential Single Family	<p>Primary Uses:</p> <ul style="list-style-type: none"> ▪ Single Family ▪ Duplex <u>Middle housing up to 3-units in a structure</u> ▪ Compatible very low-density development. <p>Secondary Uses:</p> <ul style="list-style-type: none"> ▪ Accessory Dwelling Units ▪ Agricultural uses ▪ Home based business 	<ul style="list-style-type: none"> ▪ Very low density residential suited for outlying areas taking into consideration topography and foothills aesthetics. ▪ Density: Up to 6 units per acre <u>or applicable middle housing unit density.</u> ▪ Height: 30ft
Residential Low	<p>Primary Uses:</p> <ul style="list-style-type: none"> ▪ <u>Middle housing up to 4-units in a structure</u> ▪ Compatible blended low-density housing <p>Secondary Uses:</p> <ul style="list-style-type: none"> ▪ Accessory Dwelling Units ▪ Agricultural uses ▪ Home based business 	<ul style="list-style-type: none"> ▪ Low density residential development ▪ Density: Up to 8 units per acre <u>or applicable middle housing unit density.</u> ▪ Height: 30ft
Residential Moderate	<p>Primary Uses:</p> <ul style="list-style-type: none"> ▪ Entire range of housing choices <p>Secondary Uses:</p> <ul style="list-style-type: none"> ▪ Accessory Dwelling Units ▪ Agricultural uses ▪ Home based business 	<ul style="list-style-type: none"> ▪ Moderate density residential development suitable to a variety of housing choices, subject to appropriate design and development standards for neighborhood compatibility. ▪ Density: Up to 20 units per acre <u>or applicable middle housing unit density.</u> ▪ Height: 35ft
Residential High	<p>Primary Uses:</p> <ul style="list-style-type: none"> ▪ Entire range of housing choices <p>Secondary Uses:</p> <ul style="list-style-type: none"> ▪ Adult care facilities ▪ Home based business 	<ul style="list-style-type: none"> ▪ Mixed residential densities suitable to a variety of housing choices which maintain and enhance desired urban neighborhood characteristics through appropriate design and development standards. ▪ Density: Up to 40 units per acre <u>or applicable middle housing unit density.</u> ▪ Height: 60ft

CAPITAL FACILITIES PLAN



~~CITY OF WENATCHEE 2024-2029~~ CITY OF
WENATCHEE 2025-2030

Adopted: 12/7/2023
Resolution No. 2023-34

PO Box 519
Wenatchee, WA 98807
(509) 888-6200



TABLE OF CONTENTS

Introduction..... 3

Definitions 4

CFP Organization..... 5

Service Standards..... 5

Financial Constraints..... 7

Overall Plan 8

General Facilities 8

Parks, Recreation and Cultural Services~~AND CULTURAL SERVICES~~ 9

UTILITIES 10

Stormwater 11

Water 13

Regional Water 15

Sewer 16

Transportation..... 18

Pavement Preservation..... 18

Arterial Streets 19

Street Maintenance 20

WESTERN FOOTHILLS TRANSPORTATION IMPACT FEES..... 23

Convention Center 25

Cemetery 26

~~**Homeless/Housing/CDBG Programs** 27~~

Economic Development 29

Local Revitalization Financing District – Wenatchee Waterfront 29

Economic Development/ Partnership Projects..... 30

Conclusions and Recommendation 35

Appendix A - Funded project budget sheets..... 36

Appendix B - Six Year Transportation Plan..... 37

Capital Facilities Plan

~~2024~~2025 - ~~2029~~2030

~~CITY OF WENATCHEE 2024-2029~~CITY OF WENATCHEE 2025-2030

INTRODUCTION

The City's Capital Facilities Plan (CFP) provides a comprehensive project list and schedule guiding the investment of city resources in infrastructure. These resources are made up of local revenues as well as State and Federal grants. The CFP identifies those projects which have secured funding as well as a list of projects which have not yet been funded. The projects included in this plan have largely been identified through other planning efforts which are all incorporated into the City's Comprehensive Plan. The reader is referred to these other referenced plans for additional detail concerning projects.

Why is it important for a city to keep a capital facilities plan up to date? In particular, the following three compelling reasons drive the update of the City's capital facilities plan:

1. City health and long-term stability in terms of quality of life and its economy require replacement and repair of existing infrastructure, investment in new infrastructure, and correction of deficiencies.
2. The City receives many State and Federal grants for infrastructure. For example, the majority of street improvement projects are funded with grants. The majority of infrastructure grants and loan programs require that projects be identified the City's adopted Capital Facilities Plan.
3. The Washington State Growth Management Act of 1990 requires that capital facility plans be adopted and consist of (1) an inventory of existing capital facilities; (2) a forecast of the future needs for such facilities; (3) the proposed locations and capacities of expanded or new facilities; (4) at least a six-year plan that will finance such facilities within projected funding capacities and clearly identifies sources of public money for such purposes; and (5) a requirement to reassess the land use element if funding falls short of meeting existing needs.

The purpose of the CFP is to ensure the City plans for adequate facilities that are (1) consistent with the goals and policies of the Wenatchee Area Urban Comprehensive Plan: Planning to Blossom 2037; (2) consistent with the projected population growth and land use plan; (3) concurrent with, or within 6 years of the impacts of new development in order to achieve and maintain adopted level of service standards; and (4) based on sound fiscal policies for the city.

This document is updated annually serving as the 6-year financing/implementation plan for the City's Overall Comprehensive Plan. Projects funded in the first year of the plan are intended to be consistent with the adopted city budget. The Public Facilities and Services Element of the Comprehensive Plan and referenced individual public facility plans provide the information required by RCW36.70A.070 (3).

The city-owned public capital facilities encompassed by this plan include the following:

- streets
- sidewalks, paths and trails
- parking facilities
- street and road lighting systems
- traffic signals
- domestic water system
- stormwater system
- sanitary sewer system
- parks and recreation facilities
- general administrative facilities
- museum
- convention center
- cemetery

Under the Growth Management Act, the city's Capital Facilities Plan must also be coordinated and consistent with CFP's of other public facility providers. Some of those providers within the Wenatchee Urban Area include: Wenatchee School District (schools), Chelan County P.U.D. (electrical, water, parks and recreation), Department of Transportation (highways), Wenatchee Reclamation District (irrigation water), Fire District No. 1 (fire protection), Chelan County (streets, solid waste, storm sewer, lighting, traffic signals, law and justice).

Upon passage of the annual budget for the City of Wenatchee, Appendix A will be amended representing the adopted capital projects list.

DEFINITIONS

The following definitions will help in understanding how this Capital Facilities Plan is put together and read.

Capital Facility: Capital facilities are structures, improvements, equipment, or other major assets (including land) with a useful life of at least ten (10) years. Capital improvements are projects that create, expand, or modify a capital facility. This definition applies to projects that cost more than twenty thousand (\$20,000) dollars.

Public Facility: The city-owned public capital facilities encompassed by this plan include the following: streets, sidewalks, paths and trails, parking facilities, street and road lighting systems, traffic signals, domestic water system, storm sewer system, sanitary sewer system, parks and recreation facilities, general administrative facilities, vehicles, convention center, museum, and cemetery.

Level of Service: Levels of service are usually quantifiable measures of the amount of public facilities that are provided to the community. Measures of levels of service are typically expressed as ratios of facility capacity to demand by actual or potential users. Sometimes, levels of service (LOS) standards are based on the public service, such as police protection, rather than on the facility that houses the service (e.g. police station).

Concurrency: This is a term that requires public facilities and services necessary to serve development to be in place at the time of development or a financial commitment is made to provide the facility within a certain period of time. The Growth Management Act requires concurrency on transportation facilities, while all other public facilities must be “adequate.” The Wenatchee Urban Area Comprehensive Plan, however, makes concurrency a requirement for city public facilities.

CFP ORGANIZATION

This Capital Facilities Plan is organized around each of the public facilities provided by the city. Because the city wants to make sure that the Capital Facilities Plan is based on sound fiscal policy, all capital facilities for which city funds would be expended are included in the CFP, not just those facilities required to accommodate future growth. The CFP is based on the following categories:

- General Facilities (police, administrative offices, maintenance, museum, community facilities)
- Parks and Recreation
- Stormwater
- Water
- Regional Water
- Sewer
- Street Overlay
- Arterial Streets
- Street Maintenance Projects
- Convention Center
- Cemetery
- Broadview Secondary Access / Western Foothills Circulation
- Local Revitalization Financing
- Partnership Projects / Economic Development

SERVICE STANDARDS

Under the Washington State Growth Management Act, levels of service must be adopted for Transportation Facilities as provided in the adopted transportation plans and transportation element of the Wenatchee Urban Area Plan. Adequate provisions for urban governmental services must be provided for other services in the Urban Growth Area. These standards are identified below:

Schools

The Wenatchee School District has the most and some of largest public facilities in the Wenatchee Urban Growth Area. In addition, the public schools are recognized as one of the largest public services within

the City. Education serves one of the most important roles in community health as the quality of education today impacts the skill levels, employment rates, labor supply, and wages in the future. The City's Consolidated Plan, updated in 2013, provides critical data relative to demographics, work force development, and education levels. The data clearly represents a need to improve education especially amongst the most disadvantaged children. The Wenatchee School District is responsible for facility planning and service levels of the District, but coordination with the city's overall development is to be coordinated. The WSD is currently evaluating school facilities and overall needs to serve the city and in addition, new school partnerships are developing that may result in a Charter School.

Fire Protection

City of Wenatchee is covered by Chelan County Fire Protection District #1 whose goal is to have a response of less than 6 minutes 90% of the time. Additionally, adequate fire protection for wild land urban interface events is mitigated with secondary access both for emergency vehicles and evacuations. The standard for secondary access is reflected in the Comprehensive Plan Circulation Map and building standards through the Wildland Urban Interface Code.

Police Protection

All calls for assistance will be answered within a reasonable time consistent with the nature of the call. Police staffing levels are established by Council direction based on needs for special services division or partnerships (i.e. school resource officers).

Water Supply

The City and PUD water systems are designed based on International Fire Code requirements for fire flow and Washington State Department of Health requirements for a safe supply of potable water. The City of Wenatchee is the administrator for the Regional Water System [which servesservice City of Wenatchee](#), Chelan County PUD and East Wenatchee Water District customers.

Sanitary Sewer

The sanitary sewer system is designed based on a maximum daily load demand times 2.5 for collection system. Treatment capacity is designed based on Department of Ecology requirements as detailed in the Wastewater Treatment Plant Facilities Plan.

Storm Water

The City stormwater system is designed to capture and convey runoff from Ten-year 24 hour Type 1A storm in the pipe system and twenty-five year 24 hour Type 1A storm in the streets within the gutters. Water quality best management practices are designed and operated in accordance with the current version of the *Stormwater Management Manual for Eastern Washington* published by the Washington State Department of Ecology.

Transportation, Levels of Service

The following standards are set for the city transportation network.

Automobile

LOS “E” for urban corridors of regional significance

LOS “E” – averaged in the Wenatchee Central Business District

V/C ratio < 1.0 for all other arterials, transit routes, and highways

Refer to the transportation element of the comprehensive plan, and the regional transportation plan “Transportation 2040”

Pavement Condition, Pedestrian, Bicycle, and Transit

Development of LOS and performance measures at the federal level began in 2016 and at the city level with the adoption of the complete streets policy. Future updates to this plan will include a multi-modal approach to LOS that includes non-motorized transportation functions consistent with the regional transportation plan. In 2017, the City adopted the Pavement Management Plan which identifies a pavement condition levels and corresponding investment.

All roadways on the regional system should have pedestrian facilities and proposed bicycle facilities should be funded and constructed.

FINANCIAL CONSTRAINTS

The first year of the capital facilities plan is typically consistent with the adopted City budget. However, since capital expenditures often impact multiple years after funding has been committed, it is prudent to plan ahead for the expenditure of funds for at least 3 years and in some cases longer. In addition, some of the City’s adopted plans include longer term expenditure plans in which resources are actively being pursued when opportunities arise. Often grant or loan resources become available for certain types of projects resulting from changing public policy at Federal, State, and local levels. The City of Wenatchee monitors these opportunities and puts forth funding applications for projects consistent with Federal, State, and local funding objectives. In order to effectively plan for capital expenditures, the plan is written with these concepts in mind. The following financial constraints help guide the planning process and to ensure that capital planning includes considerations of various financial impacts on the community and city government:

The first three years must be fiscally constrained. This means projects cannot be incorporated in the first three years unless they include all of the following:

- a. The project has been approved for funding from an existing source of city revenues OR grant(s) have been awarded to fund the project and match is available (awarded means the project must be on a selected funding list waiting for legislative approval or a letter of award has been received).
- b. Annual operation and maintenance costs have been accounted for, or are a reasonable increase over prior year’s expenditures
- c. If the project requires financing through the issuance of revenue bonds, voter approved bonds (UTGO), or non-voter approved/Council manic (LTGO) bonds, then Council approval of the issuance must be in place.

Years 4 and beyond represent all of the other unfunded capital needs of the City. This means although funding has not been secured, there are possible funding sources for the projects.

OVERALL PLAN

2024-2029 Project Summary

Public Facilities Type	Funded	Unfunded
General Facilities	2,000,000	2,080,000
Parks, Recreation & Cultural Services	2,053,582	2,910,000
Stormwater	14,806,753	29,676,600
Water	25,903,094	29,171,356
Regional Water	597,203	123,000,000
Sewer	13,931,783	70,850,789
Streets	256,072,116	94,372,100
Convention Center	18,335,000	-
Cemetery	-	1,170,000
Economic Development - LRF District	2,443,500	-
Economic Development - Partnerships	350,000	74,000,000
Total	\$ 336,493,031	\$ 427,230,845

GENERAL FACILITIES

Description:

General facilities cover city administrative offices, public works buildings, and other city facilities that do not have dedicated fund sources, such as water and sewer systems. The focus of the 6-year capital facilities financing plan for general facilities is the maintenance, preservation and/or expansion of city-owned facilities.

At the current downtown campus, with the addition of the new Police Station in 2004 and with the relocation of City Hall in 2022 ~~have added additional personnel which has revealed a deficiency of parking spaces~~ the existing police facility was opened up for occupancy by other departments of the City. The Information Systems Department, the Facility Maintenance Division of Public Works and the Drug Taskforce of the Police Department have all located in this facility, called the Historic Police Station. The influx of additional personnel has revealed a deficiency of parking spaces. There is a need for secured parking for the police vehicles as well as additional parking needs for the Police Station facility. Additional parking is being planned as a lid structure over a portion of the new Police Department parking lot; this project is estimated at ~~\$2,000,000~~ \$2,000,340,000. A parking analysis is needed to explore additional parking options including acquisition of additional property to determine ~~if this is~~ the best solution for the facility and the public. In addition, the city is adding parking through a public private partnership as described in the Economic Development section of this plan.

In 2018 the City completed a facilities condition assessment, which is a comprehensive look at the overall maintenance, operation and replacement needs of city owned facilities. The assessment also provides an annual cost and schedule for work to be completed. The assessment identifies \$2,200,200 of equipment that needs to be addressed. The City’s Facilities Division has evaluated the data and recommends 2023 capital improvement projects in the amount of \$400,000.

Funded Projects:

The following projects are funded and are either under way or will be underway in the near future. For additional information, please refer to Appendix A for detailed project budget sheets.

Project Name	Total Project Budget	Previous Years	2025 Budget	2026	Future Years
2322 - Wenatchee Valley Museum & Cultural Cent	2,000,000	1,000,000	1,000,000	-	-
Total	2,000,000	1,000,000	1,000,000	-	-

Unfunded Projects:

The following projects have resulted from planning and needs assessment studies or as identified by staff, community members, and elected officials. Funding has not been secured for these projects; however, possible funding sources are identified.

Project Name	Funding Sources	Est. Project Cost
Police Parking Garage	General Fund-Facilities Fund	2,000,000
Wash Bay Enclosure	General Fund-Facilities Fund	80,000
Total		2,080,000

PARKS, RECREATION AND CULTURAL SERVICES

Description:

Over a ten-month period in 2022-23 the Parks, Recreation and Cultural Services Department conducted a public involvement process to prepare an update of the six-year Parks, Recreation and Open Space (PROS) Plan. The PROS plan establishes a framework to guide the acquisition, development and improvement of park areas and facilities, habitat areas and the provision of arts and recreational services throughout the City of Wenatchee. The plan is for the 2024-2030 time period. It was recommended for adoption by the Arts, Recreation and Parks Commission at their September 2023 meeting and ~~is scheduled to be considered for adoption~~ was adopted by Resolution 2023-25 as passed by the City Council in October 2023.

The PROS plan is divided into six basic sections consistent with State requirements. The following is a summary of each section of the document to provide context for the development of the capital facilities plan. The first chapter contains a basic introduction and summary of the document. The second chapter describes the planning area including parks and recreational facilities, habitat areas, arts and current statistics. The third chapter describes the existing public, semi-public and private parks, arts and recreation resources within the planning area. The fourth chapter describes the methodology for determining the demand and needs. The wealth of recently completed, related planning efforts are

incorporated into this chapter and summarized as they relate to the City park system. The section creates a link between the City plan and State funding agency plans. The section also includes a summary of public meetings, and workshops and other outreach efforts. The fifth chapter of the plan contains the goals, objectives, and strategic actions that respond to the desires expressed during the public input process. The sixth chapter contains a summary of the capital investment plan and describes the project ranking criteria. It includes a prioritization matrix system consistent with COM recommendations. It contains a summary of common funding mechanisms, includes a section outlining ongoing maintenance considerations and arts, recreation, and organizational priorities.

For many cities, including Wenatchee, the amount of funds required to acquire the desired acres of park land and develop the proposed parks and recreational facilities is beyond their financial capabilities. Many of the capital projects included in the plan contain the assumption that some funding may be derived through future successful grant applications and continued community support in the form of financial donations. The City must continue to rely on, and partner with, other public, private, and nonprofit organizations to help generate revenue and support for the projects if they are to be realized.

Funded Projects:

The following projects are funded and are either under way or will be underway in the near future. For additional information, please refer to Appendix A for detailed project budget sheets.

Project Name	Total Project Budget	Previous Years	2025 Budget	2026	Future Years
P'Squosa Plaza Project	498,582	353,582	145,000	-	-
Bighorn Ridge Acquisition	1,505,000	-	10,000	1,495,000	
SR 285 Banners	50,000	25,000	25,000	-	-
Total	2,053,582	378,582	180,000	1,495,000	-

Unfunded Projects:

The following projects have resulted from planning and needs assessment studies or as identified by staff, community members, and elected officials. Funding has not been secured for these projects; however, possible funding sources are identified.

Project Name	Funding Sources	Est. Project Cost
Washington Park Parking, Sidewalk and Electrical	General Fund	588,000
Memorial Park Electrical and Security Lighting	General Fund, Lodging Tax Fund, Art Fund, Donations	700,000
Hale Park Phase III	General Fund	12,000
Pioneer Park ADA Access and Security Lighting	General Fund	700,000
Rotary Park Trail Overlay	General Fund	35,000
Washington Park Parking, Sidewalk, Lightng & Electrical	General Fund	500,000
Chase Park Security Lighting and Electrical	General Fund	175,000
Locomotive Park Restrooms	Sewer Fund	200,000
Total		2,910,000

UTILITIES

STORMWATER

Description:

The Stormwater Utility Fund is an enterprise fund designed to account for the financial activities related to the City's ongoing improvement and expansion of the stormwater system and compliance with the Eastern Washington Phase II Municipal Stormwater Permit issued under the National Pollutant Discharge Elimination System (NPDES) regulations. The revenue for this fund is generated from a flat monthly charge to each single-family residence as well as a monthly charge to commercial and multi-family residences based on an "equivalent residential unit". The equivalent residential unit is an impervious surface of 4,200 square feet. Part of the comprehensive plan update (2020) included a capital improvement plan along with associated rate increase recommendations. The capital improvement plan is reflected in this document.

The Stormwater capital plan includes a number of projects identified in the adopted comprehensive plan as outlined in the table below. Projects include conveyance, rehabilitation, expansion of the collection network, and water quality projects. Based on the ongoing compliance with municipal stormwater permit, many of these projects will focus on improving the quality of the water discharged into the Wenatchee and Columbia Rivers.

An important piece of the stormwater system is the Regional Decant Facility which was constructed in 2015 by the City of Wenatchee and Chelan County to provide a location to manage and dispose of street sweepings and eductor waste that would be in compliance with stormwater and solid waste management regulations. The facility is owned and operated by the City and managed under an interlocal agreement with Chelan County.

Funded Projects:

The following projects are funded and are either under way or will be underway in the near future. For additional information, please refer to Appendix A for detailed project budget sheets.

Project Name	Total Project Budget	Previous Years	2025 Budget	2026	Future Years
2012 - Snohomish & Crescent Improvements	2,328,265	141,107	-	225,000	1,962,158
2104 - Pershing (Springwater to Maple)	4,008,485	397,000	-	3,611,485	
2215 - Walla Walla Stormwater Outfall Retrofit	2,102,500	61,545	236,455		1,804,500
2310 - Roosevelt Improvements	1,680,742	13,432	160,000	1,507,310	
2403 - Public Services Center Utilities Garage/Shop	1,066,761	17,500	174,261	875,000	
2503-Olds Station Pond Remediation	1,800,000	-	300,000	1,500,000	-
Horse Lake Road (Overlook Drive to Horse Lake)	120,000		120,000		
Fifth Street Lining (Miller to Emerson)	250,000			250,000	-
Miller Street Lining (Washington to Fifth)	950,000			950,000	
Ohme Garden Road (STO3, STO4, StO5)	500,000				500,000
Total	14,806,753	630,584	990,716	8,918,795	4,266,658

Unfunded Projects:

The following projects have resulted from planning and needs assessment studies or as identified by staff, community members, and elected officials. Funding has not been secured for these projects; however, possible funding sources are identified.

Project Name	Funding Sources	Est. Project Cost
Fifth Street (Pierre to Wenatchee Ave)	Stormwater Utility Reserves	1,000,000
Millerdale (Lambert to Miller)	Transportation Improvement Board	300,000
Columbia Street(Palouse to Kittitas)	Stormwater Utility Reserves	1,800,000
Ohme Garden Road (STO6C)	Stormwater Utility Reserves	895,000
Russell	Stormwater Utility Reserves	1,020,000
Cashmere	Stormwater Utility Reserves	250,000
Cemetery Extension	Stormwater Utility Reserves	300,000
N. Wenatchee Phase 3	Stormwater Utility Reserves	650,000
Piere Street	Stormwater Utility Reserves	750,000
Upper Squilchuck Extension	Stormwater Utility Reserves	150,000
Lavern	Stormwater Utility Reserves	250,000
Vista Place	Stormwater Utility Reserves	250,000
Ramona & Sunset	Stormwater Utility Reserves	1,000,000
Loves Court	Stormwater Utility Reserves	300,000
Kenaston & Linville	Stormwater Utility Reserves	743,000
97A Conveyance (STO7, STO8, STO9)	Stormwater Utility Reserves	824,000
Western Ave	Stormwater Utility Reserves	1,604,000
Ohme Garden Road (STO1A)	Stormwater Utility Reserves	705,000
South Hills Drive	Stormwater Utility Reserves	757,000
Michael Place	Stormwater Utility Reserves	138,600
Canal/Marie/Marilyn	Stormwater Utility Reserves	344,400
South Chelan (First to Yakima)	Stormwater Utility Reserves	200,000
Day Road	Stormwater Utility Reserves	987,000
Filbeck/Seattle	Stormwater Utility Reserves	693,000
Cedarwood Lane	Stormwater Utility Reserves	138,600
Miller	Stormwater Utility Reserves	3,393,000
Skyline Drive	Stormwater Utility Reserves	2,097,000
Walnut	Stormwater Utility Reserves	1,311,000
Avenda	Stormwater Utility Reserves	221,000
Poplar	Stormwater Utility Reserves	241,500
South Miller	Stormwater Utility Reserves	283,500
Jennings St/lone and Maxine	Stormwater Utility Reserves	1,234,000
Methow	Stormwater Utility Reserves	690,000
McKittrick from Pine to Western	Stormwater Utility Reserves	2,346,000
Horse Lake Road	Stormwater Utility Reserves	1,810,000
Total		29,676,600

WATER

Description:

The major emphasis in the water distribution system over the next six years will be on replacing aging steel and cast iron water mains, removing lead parts to improve water quality and repairing and replacing reservoirs. The capital projects will help reduce the amount of Unaccounted for Water (UAW) as well as reduce emergency main line breaks, outages and damage to reconstructed roadways.

The last complete update to the Comprehensive Water System Plan adopted by the City Council in September of 2018 from which a Capital Improvement Plan (CIP) was developed. The CIP was updated more recently in 2021. Rate increases of 12% per year starting in 2022 for three years as well as increases to the system investment fees for new connections to fund the revised water capital program.

Funded Projects:

The following projects are funded and are either under way or will be underway in the near future. For additional information, please refer to Appendix A for detailed project budget sheets.

Project Name	Total Project Budget	Previous Years	2025 Budget	2026	Future Years
2202 - Crawford Water Main Replacment - Miller to Okanogar	3,865,540	204,740	3,660,800		-
2307 - Generators for Critical Utility Infrastructure	211,250		211,250		
2306 - 8 MG Reservoir Leak Repairs	1,352,600	202,600	1,150,000		
2104 - Pershing Street (Springwater to Central)	712,021	19,850		692,171	
2310 - Roosevelt Avenue (Spokane to Kittitas)	1,103,629			1,103,629	
2403 - Public Services Center Utilities Garage/Shop	1,941,761	17,500	174,261	1,750,000	
2404 - 4 Million Gallon Reservoir Replacement	16,716,293	150,000	800,000	812,000	14,954,293
Total	25,903,094	594,690	5,996,311	4,357,800	14,954,293

Unfunded Projects:

The following projects have resulted from planning and needs assessment studies or as identified by staff, community members, and elected officials. Funding has not been secured for these projects; however, possible funding sources are identified.

Project Name	Funding Sources	Est. Project Cost
Millerdale (Jessica to Miller)	Water Utility Revenues	2,815,000
Columbia (Palouse to Kittitas)	Water Utility Revenues	4,000,000
Fifth Street (Pierre to Wenatchee)	Water Utility Revenues	3,195,000
8MG & 2 MG Reservoir Exterior Coating	Water Utility Revenues	1,821,000
16' Supply to 4MG (Spokane to Okanogan)	Water Utility Revenues	3,599,000
Orondo Avenue (Washington to Alaska)	Water Utility Revenues	1,482,000
Yakima Ave (Okanogan to Alaska)	Water Utility Revenues	1,026,000
Terminal Avenue (Methow to Mission)	Water Utility Revenues	1,141,000
Cherry Street (Saddlerock to St Josephs)	Water Utility Revenues	2,244,000
First Street (Adams to Franklin)	Water Utility Revenues	810,000
North Mission Street (Fifth to Second)	Water Utility Revenues	1,068,756
South Miller (Coolidge to Idaho)	Water Utility Revenues	1,034,150
North Chelan (Fifth to Seventh)	Water Utility Revenues	925,730
South Chelan (Orondo to Spokane)	Water Utility Revenues	1,587,820
Lincoln Street (Methow to Cascade)	Water Utility Revenues	269,100
Stewart Street (Cherry to Montana)	Water Utility Revenues	208,000
Montana Street West (Stewart to Gellatly)	Water Utility Revenues	401,700
Dakota Street West (Stewart to Gellatly)	Water Utility Revenues	384,800
Washington Street West (Okanogan to Douglas)	Water Utility Revenues	400,400
Linwood/Rosewood	Water Utility Revenues	757,900
Total		29,171,356

REGIONAL WATER

Description:

The City of Wenatchee operates the Regional Water system on behalf of the Chelan County PUD, the East Wenatchee Water District and the City of Wenatchee. The Regional Water System is governed by the Regional Coordinating Committee (RCC), which is made up of representatives from each water purveyor.

In 2014 the RCC began a process to analyze the adequacy of the existing supply and transmission system to meet current and future needs and to evaluate the overall redundancy of the system. The 13 locations identified were evaluated for many criteria including water quality, wellhead protection, cost to develop, cost of operations and how they met the goal of multi-faceted redundancy. Ultimately the regional committee “short-listed” three options all south of Wenatchee, all on the Chelan County side of the Columbia River. The preferred options were determined to be non-viable. A Surface Water Treatment Plant Feasibility Study was completed in 2023. The RCC plans to continue the 10% annual rate increase in anticipation of the second source project.

The last update to the Regional Water System Plan was adopted in 2018. The plan included a capital improvement schedule as outlined in the table below.

Funded Projects:

The following projects are funded and are either under way or will be underway in the near future. For additional information, please refer to Appendix A for detailed project budget sheets.

Project Name	Total Project Budget	Previous Years	2025 Budget	2026	Future Years
2309 - Pump #2 Replacement	402,203	387,203	15,000		-
Transmission Main Corrosion Protection	195,000	-	-	195,000	-
Total	597,203	387,203	15,000	195,000	-

Unfunded Projects:

The following projects have resulted from planning and needs assessment studies or as identified by staff, community members, and elected officials. Funding has not been secured for these projects; however, possible funding sources are identified.

Project Name	Funding Sources	Est. Project Cost
Second Source Design & Construction	Regional Water Reserves / Bond	120,000,000
Redundant River Crossing	Regional Water Reserves	3,000,000
Total		123,000,000

SEWER

Description:

The City completed a Comprehensive Sewer Plan in 2017. This plan included a prioritized Capital Improvement Plan for a major extension of sanitary sewer services to provide sewer to unserved areas and support higher density development in Sunnyslope and the foothills. In 2023, the City adopted an updated Wastewater Treatment Plant Facilities Plan for projects at the plant. The plan also included an assessment of equipment and treatment units with recommendations for small works and capital projects through 2032.

Rate increases of 6% per year for five years starting in 2018, together with system investment and capital facilities fees for new connections, will support the nearly \$48M / 20-year sewer capital program. In 2022, the City received a Clean Water State Revolving Fund Loan from the Washington State Department of Ecology for the construction of Project No. 1810 – WWTP Digester 4.

Ongoing annual maintenance and rehabilitation of the collection system’s main lines, manholes and pump stations is mandatory. Specific needs are identified through a continuing and consistent video inspection program. Costs to perform this ongoing maintenance work have been included in the financial plan and revised rate structure for the sewer utility.

Funded Projects:

The following projects are funded and are either under way or will be underway in the near future. For additional information, please refer to Appendix A for detailed project budget sheets.

Project Name	Total Project Budget	Previous Years	2025 Budget	2026	Future Years
2305 - Digester Repair and Improvements (1,2,&3)	7,390,000	150,000	500,000	4,100,000	2,640,000
2307 - Generators for Critical Utility Infrastructure	211,250		211,250		
2310 - Roosevelt Utility Improvements	51,215			51,215	
2402 South Viewing Area Rehabilitation	750,000		750,000		
2403 - Administration Building/PSC Utilities Garage/Shop	4,572,918	40,000	450,000	4,082,918	
2502 - Euclid Sewer Extension - Gunn to Penny	956,400		160,000	796,400	
Total	13,931,783	190,000	2,071,250	9,030,533	2,640,000

Unfunded Projects:

The following projects have resulted from planning and needs assessment studies or as identified by staff, community members, and elected officials. Funding has not been secured for these projects; however, possible funding sources are identified.

Project Name	Funding Sources	Est. Project Cost
2308 WWTP Grit Removal	Sewer Utility Revenues	3,575,988
Biofilter Replacement	Sewer Utility Revenues	1,917,500
Fifth Street (Piere to Wenatchee Ave)	Sewer Utility Revenues	1,500,000
Lower Ohme & Highway Crossing (CSO1)	Sewer Utility Revenues	1,820,000
Olds Station (East of Euclid) Gravity Sewer Replacement (DSO2A)	Sewer Utility Revenues	1,821,000
Columbia (Palouse to Kittitas)	Sewer Utility Revenues	3,000,000
Alley Miller to Pioneer S. of Plum 8" VCP replacement	Sewer Utility Revenues	150,000
Okanogan Ave Repair & Replacement	Sewer Utility Revenues	174,000
Secondary Clarifier #3	Sewer Utility Revenues	4,785,285
Olds Station Redundant Force Main	Sewer Utility Revenues	1,000,000
South Plant Odor Control	Sewer Utility Revenues	3,039,000
Redundant Dewatering Equipment	Sewer Utility Revenues	397,000
Disinfection System Replacement	Sewer Utility Revenues	3,784,000
Ohme Lift Station and Force Main (LSO1A(3), CSO4A, CSO3A)	Sewer Utility Revenues	5,443,000
West 97A Sewer Main Extension (CSO2)	Sewer Utility Revenues	3,129,000
G3 Penny Rd. Gravity Sewer Replacement	Sewer Utility Revenues	228,064
FG01 Easy Street Gravity Sewer Trunkline	Sewer Utility Revenues	2,065,710
FG21 Skyline North (Cherry 1/2)	Sewer Utility Revenues	752,461
FG23 Methow Street - North (Craw3)	Sewer Utility Revenues	471,238
FG27 Methow Street -South (Met1)	Sewer Utility Revenues	231,000
G2 Easy Street (Olds6)	Sewer Utility Revenues	314,159
FG09 Walnut Street (Walnut1)	Sewer Utility Revenues	1,517,000
FG35 Sunnyslope Sewer Extensions	Sewer Utility Revenues	17,875,393
FG11 Harris Place (McKitt2)	Sewer Utility Revenues	324,000
G6 Methow Street (Peach2)	Sewer Utility Revenues	391,080
FG26 Circle Street – South Miller Street to Okanogan (Craw 2/4)	Sewer Utility Revenues	872,000
FG28 Canyon Breeze Lane (Met1)	Sewer Utility Revenues	333,000
G5 South Walker Avenue (Peach2)	Sewer Utility Revenues	228,000
FG08 Locust Street (Maiden4)	Sewer Utility Revenues	420,000
FG25 Circle Street – South Miller Street to Saddle Rock Trailhead (Red2)	Sewer Utility Revenues	455,000
FG32 Jeffrey Court (Squil3)	Sewer Utility Revenues	708,603
FG07 Harbel Street (Maiden4)	Sewer Utility Revenues	317,000
FG20 Lester Road, Day Road, and Day Drive (5th-2)	Sewer Utility Revenues	770,000
FG33 Chapman Road (Squil5)	Sewer Utility Revenues	1,055,876
FG10 Stella Avenue (McKitt4)	Sewer Utility Revenues	240,000
FG18 Cypress Lane (Spring1)	Sewer Utility Revenues	238,989
FG19 Westwick Road (5th-2)	Sewer Utility Revenues	725,000
FG24 Gehr Street (Craw1)	Sewer Utility Revenues	336,000
FG29 Hidden Meadow Drive (Met1)	Sewer Utility Revenues	414,000
FG30 Hidden Meadow Drive – South (Met1)	Sewer Utility Revenues	288,000
FG31 Kray Ike Court (Squil3)	Sewer Utility Revenues	315,000
FG34 Confluence Parkway Ph. I	Sewer Utility Revenues	1,134,442
FG15 Maple Street - North (Maple0)	Sewer Utility Revenues	803,000
FLS2 Warm Springs LS and FM	Sewer Utility Revenues	1,491,000
Total		70,850,789

TRANSPORTATION

PAVEMENT PRESERVATION

Description:

The Federal Highway Administration defines pavement preservation as a program employing a network level, long-term strategy that enhances pavement performance by using an integrated, cost-effective set of practices that extend pavement life, improve safety and meet motorist expectations. The following is a list of key issues related to the City's Pavement Preservation Program:

- Effective pavement preservation program will address pavements while they are still in good condition and before the onset of serious damage. By applying a cost-effective treatment at the right time, the pavement is restored almost to its original condition. The cumulative effect of systematic, successive preservation treatments is to postpone costly rehabilitation and reconstruction.
- The Street Overlay Program was developed by the Public Works Department in 1996 and was intended to provide for the overlay of all City streets over a 15-year repeating cycle
- In 2015 the City invested in a pavement management software (PMS) program to help determine the minimum funding necessary for maintaining and preserving the city's pavement system consistent with current pavement preservation methodology.
- In 2016, the City Council appointed a citizen's committee to evaluate the Pavement Management Program and recommend their preferred program and level of investment. The Committee recommended an investment of nearly \$3m per year in the Preservation Program.
- -Since 2015, the PMS program estimates have been updated on a regular basis and the current data shows that the City needs to invest \$4 Million annually to maintain an overall PCI near 70.
- Even with this level of investment, the deferred maintenance of the roadway network will continue to increase from \$1 to 2 million per year.
- The Public Works Department continues to seek ways to stretch the pavement-preservation funding by implementing lower cost surface treatments such as chip seals.
- The City currently has two dedicated revenue sources for fund #111 – Street Overlay. The two sources are the Transportation Benefit District (TBD) (sales tax of 0.1%) which generates over \$1.6M annually and the ¼% Real Estate Excise Tax (REET) which generates over \$600K per year. These two revenue sources have been providing approximately \$2.2M annually. It should be noted that the TBD includes a sunset clause for the end of 2030.
- —The City continues to seek additional funding for the preservation either through one-time cash allocations and grants.
- At the time of the latest Pavement Conditions Survey the overall pavement condition index (PCI) for our pavement network was 679.

In summary, the Cities on-going revenue allocations provide just over 50% of the needed funding to maintain the City's pavement preservation program, however the City Council continues to pursue other sources of revenue to fully fund the Pavement Management program.

ARTERIAL STREETS

Description:

Every year the City is required to adopt a comprehensive six-year Transportation Improvement Program (TIP). This plan includes financially constrained projects in the first several years and planned projects for later years. The projects are identified in the TIP as either "selected (S)" or "planned (P)" meaning that selected projects have a dedicated funding source as identified in the plan and planned projects do not. The majority of projects are financially unconstrained or planned and therefore fall into the later years of the plan. The City TIP for 2024 - 2029 was adopted by ordinance (Ordinance 2023-08) in July, 2023. **See Appendix B for a full copy of the TIP.**

The City of Wenatchee receives a portion of the State's motor vehicle fuel tax, a portion of which is dedicated to Arterial Streets. Historically, this revenue has been used as matching money for transportation infrastructure grant opportunities. Between 2006 and 2015, this dedicated funding, which averaged \$289,000 per year leveraged \$19.5 million in grant funds for street infrastructure improvements. More recent analysis has shown an even higher amount of leveraging in recent years.

Primary sources of transportation grants for the classified street system are state of Washington and Federal funds. Typical state programs include the Transportation Improvement Board (TIB), and the Active Transportation Programs. The typical Federal program is the Surface Transportation Program, [Safe Routes to School Program, Highway Safety Improvement Program, and Pedestrian and Bicycle Program](#). Both State and Federal programs include sub programs for safety and non-motorized projects. The City of Wenatchee competes with other agencies for state and federal funds. Specific criteria, including safety, mobility, structural condition, congestion, multimodal components, and project benefit/cost are often evaluated by the granting authority.

Arterial street projects are typically funded by grants with 13.5% to 20% match from the Arterial Street fund. Some programs provide incentives for a lower match percentage or have no match requirement. The TIP identifies those funded projects and associated grant funding sources. The arterial street fund has needed additional funding in the past to maintain the ability to match available grants and absorb project cost overruns. The following table presents all of the funded projects as well as projects that are currently being pursued. A comprehensive list of all of the planned City projects is included in the attached **Six Year Transportation Improvement Plan; see Appendix B**. The total capital needs identified in the 2024 TIP is approximately \$348 million.

The SR285/North Wenatchee Avenue Master Plan was adopted by the city in 2011 addressing congestion and circulation between US 2 and Miller Street. This plan was prepared by the Chelan Douglas Transportation Council (CDTC) formerly known as the Wenatchee Valley Transportation Council (WVTC) in partnership with property owners, the City, Washington State Department of Transportation (WSDOT), and Chelan County. This corridor has been identified as the highest transportation priority within the CDTC jurisdiction.

Within the Master Plan a number of options were evaluated. The Confluence Parkway alternative was approved by the CDTC as the preferred alternative which also includes a number of identified large and small projects, several on Wenatchee Avenue itself. While the City works to realize Confluence Parkway, interim improvements to North Wenatchee Avenue are necessary. Preliminary estimates for these

improvements were estimated at approximately \$55 Million. During the 2015 Legislative session the “North Wenatchee Area Improvements” project was partially funded in the Washington State Legislature Connecting Washington funding package. The project identified several components from the North Wenatchee Transportation Master Plan in its scope. These include improvements to the SR2/97 & Easy Street intersection, deployment of an intelligent Transportation System (ITS) through the North Wenatchee corridor, access control and intersection improvements at both the north and south ends of the corridor, safety improvements throughout the corridor and initiation of the environmental review for the larger Confluence Parkway project. Approximately \$17 Million was secured for the North Wenatchee Avenue area between Miller Street and US 2 through the Connecting Washington program. The WSDOT and city partnered beginning in 2017 to identify the most effective uses of the Connecting Washington funds within the North Wenatchee Avenue corridor to improve safety, mitigate congestion, improve all modes of transportation, enhance the business district, and improve the northerly gateway into the city. The partnership and investment culminated in a document titled “North Wenatchee Avenue (SR285) Preliminary Engineering Summary Report” which better defined projects and priorities for expenditure of the Connecting Washington funds allocated to the SR285 corridor. The City is looking to begin design on this work in 2024.

The city worked with the WSDOT, CDTC and regional partners beginning in 2017 to apply for federal INFRA funding to complete a suite of projects identified as the Apple Capital Loop and proposes to use the Connecting Washington funds as match. The total project size is estimated at approximately \$250 Million. In 2021, the U.S. Department of Transportation recommended \$92M in federal funding for a portion of the project which includes Confluence Parkway South. An additional \$85M in funded was appropriated to Confluence Parkway through the Move Ahead Washington Program. An Environmental Assessment was completed for the entire project in 2023.

In 2016, the city adopted a complete streets policy to ensure adequate consideration for non-motorized transportation needs along with enhanced landscaping along transportation corridors.

STREET MAINTENANCE

Description:

The City street system continues to grow through annexations and development while street maintenance funding has often not kept up. Preservation and maintenance funding levels compared to street growth and inflation have fallen behind, but pavement has been addressed in recent years as can be seen in the overlay section of this report (Pavement Management Program).

City street-related assets include pavement, curb, gutter, sidewalk, illumination, traffic signals and signs, pavement markings, bridges, and other infrastructure. Maintenance of these assets are included in the street maintenance budget 108. In recent years with the formation of the transportation benefit district and development of a pavement management program, some pavement preservation work is being accomplished by city street maintenance personnel. [Street Maintenance address streets that have been listed on the pavement condition index as differed by renting an asphalt planer to remove layers of asphalt and overlaying the travel lanes adding an estimated 10 years to the travel lanes.](#)

This section identifies some projects included in the city’s street maintenance budget that are beyond the definition of street maintenance and preservation.

Sidewalk maintenance by city code is the responsibility of the abutting property owners. A minor budget amount in the 108 fund is intended to deal with sidewalk deficiencies and minor gaps. In recent years Council has given direction to spend general and grant funds on reducing sidewalk hazards by grinding, beveling and replacing sidewalk panels. The funding also addresses accessibility by updating and adding in ramps outlined in the ADA transition plan. –The 20254 budget proposes additional funding to address sidewalk maintenance. The Transportation Benefit District was adopted with a provision dedicating ten percent of the revenues to pedestrian related improvement projects contained within the six-year street plan which could help with sidewalk rehabilitation.

Cement concrete streets are not included in the city’s pavement preservation program as their maintenance and preservation is vastly different from that of typical of asphalt roadways, and they are an important feature in our Grandview Historic District. The city has utilized maintenance funding for repairing cement concrete panels on case by case basis as they become unusable or when safety concern are raised.

Funded Projects:

The following projects are funded and are either under way or will be underway in the near future. For additional information, please refer to Appendix A for detailed project budget sheets.

Project Name	Total Project Budget	Previous Years	2025 Budget	2026	Future Years
0623 - McKittrick and Wenatchee Avenue Signal	4,129,183	4,121,183	8,000	-	-
1615 - N. Wenatchee Ave. Ped. and Median Imp's	1,397,637	1,392,637	5,000	-	-
1801 - 9th Street Rail Crossing	1,820,725	215,730	642,955	962,040	-
1919 - McKittrick St/North Columbia Street I/S	5,055,320	5,050,320	5,000	-	-
2007 - Springwater Avenue	3,841,962	3,806,962	35,000	-	-
2201.1 - Confluence Parkway South	130,849,531	20,000,000	37,000,931	50,543,000	23,305,600
2201.2 - South End Bike/Pedestrian Access Bridges	15,625,112	1,768,620	6,928,246	6,928,246	-
2201.3 - Confluence Parkway North	50,972,263	10,000,000	17,183,238	3,286,762	20,502,263
2213 - Millerdale Avenue Irrigation Canal Bridge	1,693,455	262,000	808,355	623,100	-
2302 - Worthen Street Trail Improvements	465,000	38,900	426,100	-	-
2315 - Red Apple Road	2,691,119	235,550	2,455,569	-	-
2318 N. Wenatchee Shared-Use Trail Phase 1	2,418,570	85,000	410,000	1,923,570	-
2319 - Sunset Ave Ped Improvements	628,000	61,800	81,000	485,200	-
2320 - Crawford and Okanogan Intersection Improver	1,990,500	102,500	293,000	1,595,000	-
2323 - N. Wenatchee Ave Corridor Improvements	17,374,000	800,000	2,339,420	2,143,780	12,090,800
2401 - 2025 Pavement Preservation	1,788,750	25,000	1,763,750	-	-
2407 - 9th Street Corridor Improvements	1,510,989	59,100	139,950	1,311,939	-
2408 - 1st Street Curb Ramps - Miller St to Chelan Ave	1,750,000	120,000	805,000	282,000	543,000
2501 - 2026 Pavement Preservation	2,000,000	-	305,000	1,695,000	-
2504 - 2025-2027 Pavement Condition Survey	70,000	-	70,000	-	-
Street Preservation Program	8,000,000	-	-	-	8,000,000
	256,072,116	48,145,302	71,705,514	71,779,637	64,441,663

Unfunded Projects:

The following projects have resulted from planning and needs assessment studies or as identified by staff, community members, and elected officials. Funding has not been secured for these projects; however, possible funding sources are identified.

Project Name	Funding Sources	Est. Project Cost
Pavement Preservation (Unfunded)	TBD, B&O, Util. Tax, Prop. Tax	9,600,000
Miscellaneous, Minor, Undefined	109	300,000
M&O Pavement Preservation Projects	108	450,000
M&O Sidewalk Improvements	108	300,000
M&O Pedestrian Safety Improvements	TBD, Other	300,000
Neighborhood Traffic Calming	Local, Complete Streets, Developer Funds	60,000
Illumination Upgrades	PUD, CDBG, 109, Other	60,000
Truck Route Improvements	STP	500,000
Traffic Signal Improvements	STBG, 109	1,500,000
Miscellaneous Bicycle System Improvements	TBD, Other	60,000
Washington - King - Buchanan Intersection Control	HSIP, 109	714,000
SR 285 Couplet Signal Upgrades	HSIP, 109	794,000
Idaho Street Safety Improvements	HSIP, 109	922,000
Washington Park Pedestrian Crossing	HSIP, 109	371,000
Wilson Street Safety Improvements	HSIP, 109	461,000
Millerdale Improvements	TIB, 109	4,202,000
Woodward Urban Upgrades	TIB, 109	3,860,000
South Wenatchee Avenue Complete Streets	TIB, 109	4,128,000
9th St. Pedestrian Crossing - Westside High School	SRTS, 109	467,600
Elliott Ave./Castlerock Ave. Pedestrian Crossing Improvements - Washington Elementary	SRTS, 109	632,300
Springwater Avenue Crossing - Foothills Middle School	SRTS, 109	336,200
North Road Improvements	TIF, 109	2,600,000
Maiden Lane Extension	TIF, 001	950,000
Maple, Broadview, Surry vic. Connections	Development, TIF, 001	13,049,000
Westwick extension to Maiden extension	Development, TIF, 001	7,900,000
Second Street extension west	Development, TIF, 001	1,070,000
Fifth and Western Intersection Capacity Improvements	TIF, 001	650,000
Castlerock to Fifth Street Connector	Development, TIF, 001	5,160,000
McKittrick St. - SR285 Intersection Phase 2	STBG, 109	4,300,000
North Wenatchee Ave./Hawley St/Walnut St. Intersection	WSDOT	7,000,000
North Wenatchee Avenue Complete Streets Improvements	State	1,500,000
Columbia Street South	TIF District, 109	4,500,000
Loop Trail Connection: Confluence State Park to Olds Station Road	Ped/Bike, Port, State Parks, 109	600,000
Miller/Crawford Intersection Control (Mini-Roundabout)	TIB, 109	500,000
Skyline Drive Widening	001, TIF, Other	3,500,000
Pedestrian Corridor along Stevens Street	Complete Streets, Other	250,000
Chelan/Kittitas Intersection Control (Signal)	STBG, 109	500,000
Walnut St. Improvements	TIB, 109	1,500,000
Crawford Ave. Improvements	STBG, 109	500,000
Mission Street to Mission Ridge Gateway	STBG, Other	500,000
Orondo St. Multimodal Improvements	STBG, 109	1,000,000
McKittrick St - Western Ave to City Limits	TIB, 109	1,500,000
Miller St. Realignment and Storm Repairs	STBG, 109	4,200,000
Springwater Avenue Extension North	Development, TIF, 001	1,125,000
Total		94,372,100

WESTERN FOOTHILLS TRANSPORTATION IMPACT FEES

Description:

In 2019, , a study was conducted for the Western Foothills Circulation area (within the city's UGA) for needed improvements to address adequate secondary access and level of service standards for fire access. This study was also an implementation step from the City's Community Planning Assistance for Wildfire Planning process in 2015-2016, which informed the planning effort. This study, entitled the Western Foothills Transportation Impact Study is hereby included in this plan by reference. Title 15, which was established by Ordinance 2011-02 and amended on November 14, 2019 with Ordinance 2019-41 outlines the following:

Each year staff is required to perform the following functions in order to comply with the ordinance:

1. Review the capital estimates for completion of the facilities and make adjustments to the capital estimates in the Capital Facilities Plan.
2. Review the impact fee calculation. Staff shall incorporate revised capital estimates for completion of the facilities and review the structures constructed. In addition, the impact fee ordinance provides an anticipated number of structures and lots to be created. If the cost per structure increase or decrease significantly, then staff shall make recommendations to the City Council to adjust the fees accordingly by ordinance. A construction cost inflator is contemplated in Title 15.
3. On an annual basis the City Council shall be provided with a report on the impact fee account showing the source and amount of funds collected and the public improvements financed by those funds as detailed in WCC 15.02.080. The capital facilities plan update and annual budget may serve as such report.

Public Facilities and Fees: The Western Foothills Transportation Study identified a maximum legally defensible impact fee of \$24,742 based on improvements necessary to support future development. The improvements identified in the study area consist mainly of fire access road improvements along with some multimodal improvements. Ordinance 2019-41 set the Western Foothills Transportation Impact Fee at \$7,500 per additional single-family residential Building Permit

Table 1: Cost of Basic Fire Access and Multimodal Improvements for City Connections

Connection Description	Project #	Basic Fire Access Cost	Multimodal Cost	Total Cost
North Road East of Canal	1A	\$225,000	-	\$225,000
North Road West of Canal (24-foot-wide cross-section)	1B	\$1,550,000	-	\$1,550,000
North Road Improvement through Schnibbe & Parlette Property	1C	\$825,000	-	\$825,000
Maiden Lane Extension	2	\$937,500	-	\$937,500
Maple Street Improvements	3	-	\$1,300,000	\$1,300,000
Triad Development Connections	4	\$13,049,250	-	\$13,049,250
Springwater Connection across School District Property	5	\$1,125,000	-	\$1,125,000
Westwick Connection to Triad Connection	6A	\$7,048,125	-	\$7,048,125
Westwick Multimodal Improvements to Surry Road	6B	-	\$860,600	\$860,600
Springwater (Western to Woodward)	7	-	\$2,768,400	\$2,768,400
Woodward (Springwater to 5 th Street)	8	-	\$3,626,151	\$3,626,151
2nd Street Connection, 325 ft of road across Noyes property to Philipi property	9A	\$243,750	-	\$243,750
2nd Street Connection, 775 ft of road across Philipi property from Noyes property to Maxine street	9B	\$581,250	-	\$581,250
Maxine Street Extension from Lone Place to Red Hawk Canyon Drive	9C	\$243,750	-	\$243,750
Castle Rock Connection north of water tank property to Maxine Street (on Philipi property)	10A	\$3,480,000	-	\$3,480,000
Castle Rock Connection south of water tank property to existing Castle Rock	10B	\$1,680,000	-	\$1,680,000
5 th Street & Western Avenue Intersection Improvements*	11	-	\$632,100	\$632,100
Total		\$30,988,625	\$9,187,251	\$40,175,876

Source: City of Wenatchee

*This project is an LOS-driven intersection project

CONVENTION CENTER

Description:

The Convention Center is a City-owned facility that is operated under agreement with Coast Hospitality, LLC (formerly Coast Wenatchee). Not only is the Convention Center an anchor facility to Wenatchee’s Historic Downtown, but it is particularly important to the City’s economy. The success of the Convention Center has a direct impact on lodging and food service businesses and therefore, revenue from the operation of the facility and revenue from lodging taxes from hotels within the City are used to operate and maintain the facility as well as pay off long-term capital debt and provide for facility and equipment upgrades and replacements. This funding is reviewed and approved during the annual application process carried out by the City’s Lodging Tax Advisory Committee.

The original Convention Center was built and equipped in 1980 and has had many renovations throughout the years to keep the facility competitive. With guidance from the Lodging Tax Advisory Committee, lodging tax funds have been directed to continue facility upgrades and pay off existing capital bonds.

In 2019, the City commissioned E.D. Hovee & Company LLC to survey market conditions and opportunities associated with the WCC. Based on the conclusions of the Hovee report the Tourism Promotion Area Committee, the Local Tax Advisory Committee, and the City Council recommended the City began the design process for the renovations and/or expansion. The design focus goal is to make the WCC the “best little conference center in Eastern, WA.” Additional purpose is to accommodate more adequate vendor and ballroom space, provide better accessibility for visitors, facilitate ease of coordination through the design with the Performing Arts Center, maximize all indoor and outdoor spaces, and upgrade existing technology. The design was completed in 2023, with construction slated to begin in 2024 and completion in 2025. The total project budget is \$16,140,000.

Most recent projects include roof replacement, six AV meeting systems for remote meetings, fire alarm panel upgrades, the refurbishment of room airwall dividers, additional chair railing installation and refinishing of the main entrance stairway.

Funded Projects:

The following projects are funded and are either under way or will be underway in the near future. For additional information, please refer to Appendix A for detailed project budget sheets.

Project Name	Total Project Budget	Previous Years	2025 Budget	2026	Future Years
2212 - Wenatchee Convention Center Renovation	17,535,000	6,295,000	11,240,000		0
Minor Capital / Equipment (\$200,000/yr)	800,000	200,000	200,000	200,000	200,000
Total	18,335,000	6,495,000	11,440,000	200,000	200,000

CEMETERY

Description:

Over the past 10 years, improvements were implemented including the construction of a Niche Wall and new office, re-platting of several areas, removal of roadways and renovation of the Clark Mausoleum. Additionally, rate adjustments have been made with the goal of helping to enhance revenues. However, even with the rate changes, overall revenues have not kept pace with the modest rate of increased expenditures and the Cemetery Fund has continued to rely on General Fund transfers in order to continue to operate.

The Cemetery capital plan includes additional improvements to the Home of Peace Mausoleum on Miller Street and improvements at the Cemetery including crypt walls, roadway drainage improvements, greenhouse and shop upgrades, addition of section signs and an informational kiosk, Veterans Section improvements, and eventually the opening of a new full interment section with the potential for double depth lots. The Cemetery is currently nearing capacity for available graves and niche capacity is at approximately forty percent. Without constructing the new roadway and opening Section M, the Cemetery will no longer be able to accommodate burials in the near future. In 2023, engineering was prepared to construct the roadway when funding is available for construction.

Funded Projects:

Project Name	Total Project Budget	Previous Years	2024 Budget	2025	Future Years
Area Identification Signs	25,000	-	25,000		
Total	25,000	-	25,000	-	-

Unfunded Projects:

Project Name	Funding Sources	Est. Project Cost
Storm Water System	General Fund/Sotrm Water	350,000
Irrigation System Upgrade	General Fund	220,000
Shop/Greenhouse Replacement	General Fund	120,000
Cemetery Expansion - Niche walls	General Fund	80,000
Cemetery Road for Section M	General Fund	400,000
Total		1,170,000

HOMELESS/HOUSING/CDBG PROGRAMS

Description:

The City of Wenatchee administers homeless and affordable housing programs that serve both Wenatchee and East Wenatchee. These programs are funded through contributions by the Cities. Funding is received from four main sources in addition to non-reoccurring grant resources:

- ~~1. The Wenatchee/East Wenatchee Affordable Housing Sales Tax Fund is generated through local sales tax collections authorized by RCW 82.14.540 (HB 1070). Annual revenue fluctuates depending on the economic cycle but averages approximately \$1.8 million/year.~~
- ~~2. The City of Wenatchee Local Homeless Fund (HB 2163) is generated through local document recording fees. Annual revenue fluctuates depending on the number of documents recorded in Chelan County but average approximately \$160,000/year.~~
- ~~3. The City of Wenatchee Affordable Housing Sales Tax Credit is generated through a local sales tax credit against the state's sales tax collections authorized by RCW 82.14.540 (HB 1406). Annual revenue fluctuates depending on the economic cycle but averages approximately \$85,000/year.~~

The City typically passes these funds through to partner agencies that operate homeless service programs such as temporary low barrier emergency shelters, homeless outreach, landlord liaison services, and permanent supportive housing programs. Occasionally, these funds help construct or purchase capital facilities and equipment and thus this description is included in the Capital Facilities Plan. The City of Wenatchee is pursuing the implementation of the Chelan — Douglas Homeless Housing Strategic Plan 2019-2024. A multi-phased strategy has been developed by the City of Wenatchee and East Wenatchee to look at having a significant positive impact on providing shelter and wrap around services for the unsheltered homeless population. This initial first phase includes the development of capacity for new low barrier shelter(s) and an integrated approach at outreach with local law enforcement focused on the unsheltered homeless population.

The first phase of this development includes two RV and personal vehicle day park low barrier emergency shelters, in addition to a low barrier emergency shelter with approximately 44 individual Pallet Shelters, 88 beds, at the Wenatchee Rescue Mission. These facilities each will include wrap around services through the local continuum of care. Final development of improvements for the two RV and personal vehicle day parks will be complete by December 2023. Site planning and construction for the 44 individual Pallet Shelters began in 2023 and may be complete by December 2023. Should weather or unforeseen factors arise, the site construction and placement of the units would be completed in the Spring of 2024. One of the units, an emergency unit established for families, will not be placed until 2024 due to a potential soil remediation process. Two additional low barrier emergency shelter facilities were granted funds in 2022 with a combined bed count of 74 units.

Funded Phase 1 Projects:

Project Name	Funding Sources	Est. Project Cost
Low Barrier Emergency Shelter	Affordable Housing Sales Tax (HB 1590) & Low Barrier Shelter Grant	
Total		\$931,800

Community Development Block Grant (CDBG)

In 2005, the City became a CDBG Entitlement Community. CDBG funds are used for grant administration and staff time, public service programs, small-scale neighborhood revitalization projects, improvements to public facilities and large public infrastructure projects. The following goals were identified for the 2020-2024 Consolidated Plan which identifies priorities for CDBG investment in the community:

1. Expand Development of Housing & Public Amenities
2. Enhance Economic Development
3. Support Public Services

The City of Wenatchee is eligible to receive Community Development Block Grant (CDBG) funds annually from the U.S. Department of Housing & Urban Development (HUD). The City’s program year is October 1st through September 30th of the following calendar year.

Funding from the CDBG program is able to be utilized within the geographic boundaries of the City of Wenatchee. In order to serve community members in need, geographic priority areas are created. The characteristics within a priority area include higher levels of crime; housing stock in a state of deterioration; lack of public infrastructure and facilities; high levels of code enforcement contact; and signs of potential blight. Additional considerations include access to public facilities, commercial areas, grocery stores, recreation and medical or social support facilities.

Area-benefit activities are emphasized to benefit low- and moderate-income neighborhoods and limited clientele are more specific to support households. The City has successfully leveraged CDBG funding for Safe Routes to School projects and coordinates efforts to align with the “Five Year Plan to Reduce Homelessness in Chelan & Douglas Counties” around housing needs. The following table illustrates a list of projects consistent with the 2020 – 2024 City of Wenatchee Consolidated Plan. Public facilities and infrastructure improvements such as the Lincoln Park redevelopment and the extension and replacement of sidewalks are proposed.

Funded Projects:

Project Name	Funding Sources	Est. Project Cost
Public Facilities and Infrastructure	2023 CDBG Grant and additional public/private resources	346,926
Total		687,423

ECONOMIC DEVELOPMENT

LOCAL REVITALIZATION FINANCING DISTRICT – WENATCHEE WATERFRONT

Description:

The city adopted the waterfront subarea plan in 2004. This redevelopment and revitalization plan focuses on changing the character of the waterfront to compliment the waterfront parks through mixed use development. The city has been very active in furthering this plan through the investment in public infrastructure. The city continues to advocate for the development of the waterfront and furtherance of the objectives of the plan.

~~The City was awarded a state rebate in 2009 presenting opportunities to fund and complete additional projects on the waterfront utilizing the State Local Revitalization Financing (tax increment financing) program. In concert with this award, ordinance 2009-26 established the local revitalization district (District) entitled “Wenatchee Waterfront” which includes the area bounded by the Columbia River to the east and the railroad tracks and Walla Walla Avenue to the west. The District is bounded by Thurston Street to the South and Walla Walla Park to the North. These projects must be used for public improvements that stimulate economic growth within the District. The ordinance was amended in 2013 with ordinance 2013-14 to add eligible projects. It is anticipated that the ordinance will need to be modified again to accomplish other projects identified in the waterfront subarea plan or projects that arise that will directly support redevelopment. Several projects associated with the Pybus Market Charitable Foundation, Port of Chelan County, and the Chelan County PUD have already been completed and the City is currently using LRF proceeds to pay down associated debt. Projects completed to date include the Wastewater Treatment Plant odor and visual mitigation, the Pybus Public Market, purchase of the property underlying and adjacent to the Pybus Market, parking behind the public market, improvements to the PUD access road, park expansion at the former Public Works site, improvements to Worthen Street, the Worthen/Orondo stairs project, the Orondo Street plaza, the Ninth Street Parking Lot. The sales tax annual rebate of up to \$500,000 will be received by the City until year 2037. The rebate may only be used to pay for debt on public improvement projects. The City issued a General Obligation Bond prior in 2016 to refinance and maximize the use of LRF funds. Some of the following projects are taken from directly from the waterfront subarea plan if they can be used to incentivize redevelopment. Other projects have been developed based on needs that have arisen due to successful development, such as parking around the Pybus Market area. This parking has been instrumental in furthering economic growth in the South Node with the addition of the Hilton on the former Public Works property in 2019. Included in the list of projects is a development support fund that could be used for public private partnerships where public improvements are needed utilizing pay as you go property tax increment that will continue to be revenue for the District after all the bond proceeds have been expended. The LRF statutes authorize expenditures of local increment broadly in support of redevelopment.~~

Funded Projects:

The following projects are funded and are either under way or will be underway in the near future. For additional information, please refer to Appendix A for detailed project budget sheets.

Project Name	Total Project Budget	Previous Years	2025 Budget	2026	Future Years
2317 - Pybus South Parking Lot	2,443,500	50,000	92,000	671,000	1,630,500
Total	2,443,500	50,000	92,000	671,000	1,630,500

ECONOMIC DEVELOPMENT/ PARTNERSHIP PROJECTS

Description:

Economic Development is identified in the Comprehensive Plan as a community goal to enhance the quality of life of citizens. In addition, the Wenatchee Valley serves as a regional commerce hub for North Central Washington and portions of the Columbia Basin. Beginning early in 2000, the City began to see major sales tax generators either moving out of the city or locating primarily in East Wenatchee and Douglas County. Most of the growth for these large sales tax generators is expected to occur where land is plentiful outside the City limits. As a result, economic development efforts have become a focus of the City in order to sustain public services and to shape our community as the city experiences dynamic growth over the next 10 -15 years. Wenatchee is one of many cities that is targeting economic development through public private partnerships and/or through strategic investments from public agencies working together. The Pybus Public Market is a great example of a partnership project. From time to time, there may be a need to cooperate on community facilities or economic development projects that provide a regional benefit or projects that fall outside of the traditional infrastructure projects anticipated for a City. ~~The City has been successful in implementing the projects funded through Local Revitalization Funding and is now looking towards other funding opportunities such as tax increment financing, grants, and partnerships with private and public agencies. A number of projects listed below are aligned with the goal of enhancing the quality of life and securing the City's position as a regional commerce hub: For example, with the 2017 tax act, opportunity zones were created to replace the New Market Tax Credit system. Opportunity zones are designed to spur investment in designated low income tracts. Wenatchee was awarded two opportunity zones which includes all of downtown and south Wenatchee. Public private partnerships are anticipated to accompany investments using the Opportunity Zones. A number of the projects listed below are located in opportunity zones.~~

1. The City, in partnership with the Federal Highways Administration and Washington State Department of Transportation formally initiated NEPA for Confluence Parkway in 2019 ~~and concluded this process with a Finding of No Significant Impact in 2023. Subsequently, the City applied for and was awarded \$92 million in Infrastructure for Rebuilding America (INFRA) funds and \$85 million from the Washington State Move Ahead Washington effort. This effort is required to further the project and secure funding as outlined in the Chelan Douglas Transportation Council and City Transportation Plans. NEPA is anticipated to be completed through development of an Environmental Assessment which will identify mitigation measures for environmental impacts. The city is funding \$3 M for this effort including funds from the State of Washington, Link Transit, the Chelan Douglas Regional Port, and Chelan County Public Utilities District. Confluence Parkway and the Apple Capital Loop network continues to be a priority for establishing the infrastructure and transportation network that is necessary to support the growth of Wenatchee.~~

- ~~2. The City partnered with WSU to perform an Integrated Planning Grant funded by the Department of Ecology for approximately 9 acres of property at the WSU Treefruit Research and Extension Center located at the corner of Western and Springwater Avenues. The objective of the grant was to determine if the property is encumbered by contamination and develop a plan for the property that considers housing that would support research and education related jobs in our valley. Lead Arsenic contamination was found on the site and thus opportunities are available through the Department of Ecology to support cleanup and housing projects. This project is intended to help further a 4 year university presence in the Wenatchee Valley through investment in the WSU Treefruit Research and Extension Center. The project is also coordinated closely with the Our Valley Our Future game changer, the Bridge Innovation District. Given housing is a critical need for professionals, students, researchers, and interns, this project has the potential of providing critical housing in proximity to the WSU campus. The City anticipates facilitating a project through facilitating cleanup of the site and development of approximately 100 units of housing. No general fund expenditures are expected; however, grant resources and partnerships with organizations such as the housing authority may provide the opportunity for facilitation of this project.~~
2. During 2016, the City invested \$15,000 on behalf of the region to address a burgeoning housing crisis. The study found that there is a lack of market rate housing stock and a housing mismatch in terms of housing sizes. The result of this deficiency is inflated housing prices and down renting which is placing considerable pressure on units that should be affordable. Thus, the study recommended a number of actions to help facilitate the construction of both market rate housing and affordable housing. Investing in capital improvements identified in this plan is recommended to increase the supply of readily developable land. The city anticipates using the general fund and utilities to assist in preparing infrastructure to support development of market rate and affordable housing. Market rate housing is easier to achieve in the short-term and is a benefit for all housing affordability ranges. Long range efforts are also needed to support the development of subsidized and affordable housing stock. Partnerships with non-profit entities that utilize State and Federal tax credits and incentives create the best opportunities for developing projects that will supply more of housing.
3. In 2024, at the request of downtown Stakeholders, the City engaged in a planning process to reimagine the downtown, waterfront and warehouse district and plan for future improvements. The Reimagine Wenatchee Target Area Master Plan identified a plan for making strategic investments that will increase economic vitality and enable a vibrant, sustainable live, work and play environment. The Plan prioritized nine design and implementation strategies to consider including five capital projects that will better support the target area including Columbia Street Public Realm Improvements, Mission and Chelan Pedestrian Improvements, Mission and Chelan One-Way to Two-Way Couplet Conversion, Pedestrian Bridge over BNSF tracks, and a new Parking Hub over the existing Wastewater Facility at Worthen. It needs to be noted that many of these capital projects need further study and discussion before implementation.
4. The city entered into a public private partnership with Mission and Kittitas Apartments, LLC (Weidner Development) through a development agreement in 2019 to provide approximately 225 market rate housing units in 5 levels of housing over 2 levels of parking format. The city agreed

to lease 100 stalls of parking for public purposes to support parking in downtown and housing development for the economic health and vitality of downtown and South Wenatchee. This project will also serve as one of the first transit-oriented development projects given its proximity to Columbia Station. The city lease will begin in 2023 or 2024 after the project is complete. Additionally, the City is working with Weidner Development to upgrade the existing streetscape leading into and adjacent to the new multi-family development. The proposed streetscape project will expand sidewalks, upgrade lighting, upgrade utility infrastructure, and install enhanced landscaping in a two block area from Orondo to Kittitas.

5. The City works closely with the Wenatchee Downtown Association implementing projects in the adopted Central Business District subarea plan which have included improvements to downtown parking, streetscapes (LID), utility improvements, truck traffic re-rerouting, bike facilities, public space improvements, the development of a new housing project (Mission and Kittitas Apartments, LLC), redevelopment of the vacant upper stories of historic buildings, improvements to the Convention Center as discussed earlier and development of connections to the South Node of the Waterfront. Parking continues to be the biggest management issue in order to realize a revitalized downtown area. As a result, the City performed a downtown parking study in 2016. This parking study analyzed existing parking capacity, usage, and current opportunities to better utilize parking. The study also identified management strategies for areas of surplus supply and deficiencies of parking in the downtown and South node of the waterfront. In general, the plan identified a surplus of parking in the study area equating to approximately 1,500 stalls. At the same time, the study illustrated a need for additional parking in the area of the Convention Center and Public Market due to high peak demands. The study suggests the city focus on management of the on street system and encourage redevelopment of surface lots to increase business activity until 70-85% utilization rates are achieved. This means that the city will need to maximize on-street parking and discourage surface lots. This plan identifies a funding estimate to increase on street supply and additional funding for off street parking for city employees within the Police Station and City Hall in the General Facilities Section. Partnership projects will likely arise in the development of public private partners for shared parking of underutilized city and private parking lots.
6. A long-term goal of the City has been to facilitate the relocation of the Burlington Northern Santa Fe (BNSF) switch terminal located at the intersection of Columbia and Thurston Streets to the Appleyard. Development of the Pybus Public Market has demonstrated to the community the importance of this project. Presently, BNSF uses the terminal to service trains with crew changes. During periods of train service, trains park on the tracks often for periods of more than one hour at a time. In 2013, the Orondo crossing was monitored for several days to verify train patterns. Upon discussion with BNSF staff, train traffic is expected to increase with the improving economy. With increased activity on the waterfront and these blockages of specifically the Orondo and Ninth Street crossings, there is a community desire to address the problem. The Chamber of Commerce, Wenatchee Downtown Association, and Pybus Market requested that the City take a closer look at relocation options. In 2014, the City partnered with Chelan County, Chelan County Port District, Link Transit, the Chelan Douglas Transportation Council, Wenatchee Downtown Association, Wenatchee Valley Chamber, and the Pybus Market to raise \$35,000 for a relocation study. BNSF presented the results of this study in March of 2015. The study provided results that were different from originally anticipated in that it recommended moving the switching operation to a new 22,500 foot siding in Malaga. The total cost of this project is estimated at \$32 Million.

This revised outcome not only accomplishes the original goal of preventing blockage of the crossings in Wenatchee, but also addresses access problems along the Malaga Waterfront. This project could have the added benefit of opening up 190 acres for development in Malaga. Another option may be to relocate the facility to Douglas County near the Mouth of Moses Coulee in partnership with Chelan County and the Transportation Council. It is noted that BNSF is interested in this project and has been a proactive partner.

7. During the summer of 2015, the Sleepy Hollow Fires burned 3 major industrial facilities in North Wenatchee. In order to facilitate the best redevelopment opportunities, the City developed a Master Plan for approximately 50 acres of the impacted area during 2016. This area also includes the 7.5-acre Washington State Department of Transportation (WSDOT) Administrative offices property which was vacated in 2018 and the buildings demolished in 2021. ~~The Master Plan identifies road and utility work totaling \$37 Million which will enable an estimated \$245 Million in private investments including up to 850 residential units, office, light industry flex space, and retail space along Wenatchee Avenue. The city has been working to secure properties and right of way through early acquisition to facilitate redevelopment in the area. Community Economic Revitalization funding was received to establish the McKittrick Street right of way in partnership with Stemilt growers and location of the Diamond Foundry; tThe Ceity acquired approximately 8 acres of property including the former WSDOT property and has been actively working on marketing the property to the private market for development as envisioned in the North Wenatchee Master Plan. will sell remnant properties to encourage private investment.~~ In addition, the city ~~will~~ has been awarded federal funding for major elements of this work including the McKittrick Street and Miller Street underpasses that will tie North Wenatchee Avenue improvements in with Confluence Parkway. The City has continued to endeavor to identify additional funds to support this and other redevelopment effort, including tax increment financing districts (TIF). The City implemented a TIF district for the North Wenatchee Redevelopment area in late 2022. Note that most of the infrastructure work associated with this redevelopment is identified in other sections of this plan.

- ~~8. A national campaign to develop makerspaces to foster innovation in manufacturing and promote workforce development has been underway since the Mayor entered the National Mayor's Maker Challenge in 2015. In 2019, the city engaged in a National Recreation to Technology challenge in 2019 to help facilitate how to develop makerspaces in Wenatchee through community partnerships. This effort was made in partnership with the Pinnacles Prep. Charter School, the Wenatchee Public Library, GWATA, and other partners. While makerspaces are not typically owned and operated by cities, the capitalization requirements for the development of a space may require a public private partnership. The City may also help facilitate the acquisition of grants to help develop and achieve this emerging goal.~~

- 9.8. In October of 2012, a Sustainable Design Assessment Team sponsored by the American Institute of Architects visited South Wenatchee to perform an assessment and make recommendations of what could be done to improve South Wenatchee. The team developed a report suggesting transportation improvements, public art ideas, neighborhood enhancements, and economic redevelopment suggestions. The community followed this process with the development of a subarea plan. The South Wenatchee Action plan was completed in 2016 and adopted in 2017 as part of the city's Comprehensive Plan. The priorities in the plan include basic infrastructure around sidewalks, lighting, and parks. In addition, the plan includes a number of elements to grow

and improve the South Wenatchee business district, such as extension of the Columbia River Pipeline Bridge and connection to the Apple Capital Loop Trail which was awarded INFRA funding and is currently in design. ~~Another example is the development of a food truck plaza honoring the cultural diversity in South Wenatchee. This section of the plan includes by reference a number of projects that support workforce development, entrepreneurship, education, business growth, and investment in this portion of the city. The city’s purchase of a portion of the Federal Building to house City Hall is an example of one such project. The City recently engaged the business community along South Wenatchee Avenue for feedback and input on the future design of that corridor. The new South Wenatchee corridor envisions pedestrian amenities and business-friendly upgrades such as more off street parking.~~

The following table capture potential city partnership matches to projects that are already included in the preceding sections of this plan to avoid double counting projects.

Funded Projects:

The following projects are funded and are either under way or will be underway in the near future. For additional information, please refer to Appendix A for detailed project budget sheets.

Project Name	Total Project Budget	Previous Years	2025 Budget	2026	Future Years
Mission & Kittitas Downtown Housing /Parking	350,000	-	350,000	-	350,000
Total	350,000	-	350,000	-	350,000

Unfunded Projects:

The following projects have resulted from planning and needs assessment studies or as identified by staff, community members, and elected officials. Funding has not been secured for these projects; however, possible funding sources are identified.

Project Name	Funding Sources	Est. Project Cost
Reimagine Wenatchee Plan	Tax increment financing, Grant, Public Private Partnerships	30,000,000
Downtown Housing Projects	State Affordable Housing Funds, Public Private Partnership	2,000,000
Downtown Parking/Garages	Parking Revenues, LID, General Fund, Public Private Partnerships	3,000,000
Burlington Northern Terminal Relocation	Federal Grants, BNSF	32,000,000
North Wenatchee Master Plan Infrastructure	State grants and loans, public private parnterships	2,000,000
South Wenatchee Subarea Plan	State and Fed. Grants, Gen. Fund, Util, Community Grants, Other	5,000,000
Total		74,000,000

CONCLUSIONS AND RECOMMENDATION

The total value of the capital improvements identified in this plan is over \$640 Million. The distribution of identified needs make it difficult to prioritize where the limited funds the city has for capital should be applied. The city has historically relied on grants for much of the infrastructure especially related to road, parks, and economic development related improvements, and has been very successful in obtaining them. General facilities maintenance capital and new facilities are the hardest to fund and often require issuance of bonds or the use of reserves to make improvements. As the city looks forward, its continued creativity and use of tools such as tax increment financing, private partnerships, tax credit incentive programs, and local tax options will continue to be needed. Having a good understanding of public policy, developing implementation plans, and being highly proactive in discovering and using these tools will help address the city's capital needs. It is the intent of this capital facilities plan to prioritize and look for creative solutions to support the City's Urban Area Comprehensive Plan and stay concurrent with the governing laws of the city, state, and nation.

APPENDIX A - FUNDED PROJECT BUDGET SHEETS

APPENDIX B - SIX YEAR TRANSPORTATION PLAN